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BB6	BRIDGE NO. 5/40W - FURTHER DECK PREPARATION PLAN
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-	- BB16 - BB24 TC3

SHEET NO.	PLAN REFERENCE NO.	TITLE

PLAN
REFERENCE
NO.
SHEET
OF
SHEETS

NOTE: ALL SHEET REFERENCES, FIRST NOS. OF STRUCTURE CODE DESIGNATIONS AND MATCH LINE SHEET REFERENCES, ETC., THROUGHOUT THE PLANS, REFER TO THE ENTRY IN THE PLAN REFERENCE NUMBER BOX.

REGIONAL ADM.		REVISION	DATE	BY		AL63/9, AL6360	DATE P.E. STAMP BOX	DATE P.E. STAMP BOX		INDLX	SHEETS
PROJ. ENGR.	S. FELL				1	XL6379, XL6380			-	INDEX	84
CHECKED BY	A. ROMANCHOCK				CONTRACT NO.	LOCATION NO.			Department of Transportation		OF
ENTERED BY	CCAEO				22X333				3		5
DESIGNED BY	T. PANKAU				JOB NUMBER				Washington State	DECK REPAIR / OVERLAY	SHEET
PLOTTED BY	hillb				IU WASI]				NB AND SB LEWIS RVR BR	"172
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FILE NAME	\\Wsdot.loc\sw\Group\Englnee	rlng\444301\05-Design\02-Projects\I-5 NB & SB Lewis R	ver Br and	NB To	outle River Br_	Deck Repair\1-CADD-Plans_Cor	rtractPlans\001_22X333_PS_IN.dg)			Plot 1

PROJECT LICENSED PROFESSIONAL CERTIFICATES

lumbell	Fireffel	Geoffrey D. Jurillo	Mahel P. Jelian
Susan Fell	LisaRene Schilperoort	Geoffrey D Swett	Richard Zeldenrust
May 2, 2023	May 2, 2023	May 2, 2023	May 2, 2023
AS A LICENSED PROFESSIONAL IN DIRECT RESPONSIBLE CHARGE OF DEVELOPING THIS CONTRACT, I CERTIFY THAT ALL PLANS THAT CONTAIN MY STAMP HAVE BEEN DEVELOPED UNDER MY SUPERVISION.	AS A LICENSED PROFESSIONAL IN DIRECT RESPONSIBLE CHARGE OF DEVELOPING THIS CONTRACT, I CERTIFY THAT ALL PLANS THAT CONTAIN MY STAMP HAVE BEEN DEVELOPED UNDER MY SUPERVISION.	AS A LICENSED PROFESSIONAL IN DIRECT RESPONSIBLE CHARGE OF DEVELOPING THIS CONTRACT, I CERTIFY THAT ALL PLANS THAT CONTAIN MY STAMP HAVE BEEN DEVELOPED UNDER MY SUPERVISION.	AS A LICENSED PROFESSIONAL IN DIRECT RESPONSIBLE CHARGE OF DEVELOPING THIS CONTRACT, I CERTIFY THAT ALL PLANS THAT CONTAIN MY STAMP HAVE BEEN DEVELOPED UNDER MY SUPERVISION.
Genmiao Chen			
Genmiao Chen (May 2, 2023 08:13 PDT)			
Genmiao Chen			
May 2, 2023			
AS A LICENSED PROFESSIONAL IN DIRECT RESPONSIBLE CHARGE OF DEVELOPING THIS CONTRACT, I CERTIFY THAT ALL PLANS THAT CONTAIN MY STAMP HAVE BEEN DEVELOPED UNDER MY SUPERVISION.	AS A LICENSED PROFESSIONAL IN DIRECT RESPONSIBLE CHARGE OF DEVELOPING THIS CONTRACT, I CERTIFY THAT ALL PLANS THAT CONTAIN MY STAMP HAVE BEEN DEVELOPED UNDER MY SUPERVISION.	AS A LICENSED PROFESSIONAL IN DIRECT RESPONSIBLE CHARGE OF DEVELOPING THIS CONTRACT, I CERTIFY THAT ALL PLANS THAT CONTAIN MY STAMP HAVE BEEN DEVELOPED UNDER MY SUPERVISION.	AS A LICENSED PROFESSIONAL IN DIRECT RESPONSIBLE CHARGE OF DEVELOPING THIS CONTRACT, I CERTIFY THAT ALL PLANS THAT CONTAIN MY STAMP HAVE BEEN DEVELOPED UNDER MY SUPERVISION.
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NOTES:

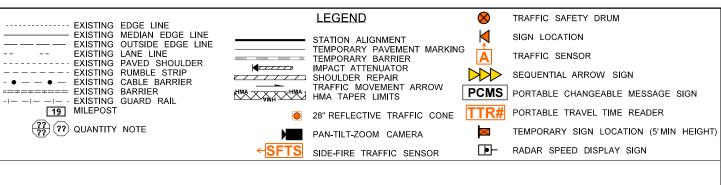
THIS PLAN SET WAS DEVELOPED ELECTRONICALLY UNDER THE DIRECT SUPERVISION OF THE LICENSED PROFESSIONALS THAT HAVE AFFIXED THEIR SIGNATURE TO THIS PAGE.

THIS SHEET SERVES AS THE CERTIFICATION BY THE ABOVE LICENSED PROFESSIONALS OF ALL SHEETS IN THIS PLAN SET WHERE THEIR STAMPS AND SIGNATURES APPEAR.

FILE NAME	\\Wsdot.loc\sw\Group\EngIneering\444301\05-DesIgn\02-Projects\I-5 NB & SB LewIs R	Iver Brand N	IB Toutle River E	3r_Deck Repair\1-CADD-Plans_Co	rtractPlans\002_22X333_PS_CT.dg	n			
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DESIGNED BY	T. PANKAU		JOB NUMBER	· · · · · ·			Washington State	DECK REPAIR / OVERLAY	SHEET
ENTERED BY	CLARK COUNTY AEO		22X33	3			3	DEGIT RELATION TO VERLEAT	6
CHECKED BY	A. ROMANCHOCK		CONTRACT N	D. LOCATION NO.			Department of Transportation		OF_
PROJ. ENGR.	S. FELL			XL6379, XL6380	DATE	DATE		CERTIFICATION SHEET	84 SHEETS
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NOTE: THE FIRST NUMBER OF THE "CODE" BELOW REFERS TO THE SHEET NO. OR THE SHEET REFERENCE NO. SHOWING THE CONSTRUCTION FEATURE. THE SECOND NUMBER REFERS TO THE CONSTRUCTION FEATURE FOUND ON THAT SHEET.	REMOVING PLASTIC LINE	REMOVING RAISED PAVEMENT MARKER	PLANING BITUMINOUS	HMA CL. 3/8" PG58V-22		TEMPORARY BARRIER	BARRIER DELINEATOR	RAISED PAVEMENT MARKER TYPE 2	TEMPORARY PAVEMENT MARKING - LONG DURATION			SEE GENERAL NOTES	
CODE LOCATION Y \ UNIT OF MEASURE >	L.F.	HUND	S.Y.	TON		Ĺ.F.	EACH	HUND	L.F.				
**** STAGE A QUANTITIES **** SU4-1 N 73+33 TO 94+51 (36' RT) SU4-2 N 73+33 TO 95+31 (24' RT) SU4-3 N 73+33 TO 95+31 (12' RT)	2120 2200 2200	0.6 0.6 0.6											PLANING BITUMINOUS PAVEMENT FOR REMOVING RUMBLE STRIPS AT 0.15' IN DEPTH AND AT A WIDTH OF 40". 2. PLANING BITUMINOUS PAVEMENT FOR
SU4-4 N 73+33 TO 94+51 (0') SU4-5 N 73+33 TO 95+31 (24' RT)	2120	0.6							2200			6	SHOULDER REPAIRS ASSUMES FULL DEPTH REMOVAL AT A WIDTH OF 6'. HMA FOR
SU4-6 N 73+33 TO 95+31 (12' RT) SU4-7 N 73+33 TO 94+51 (0')									2200 2120			6	SHOULDER REPAIRS TO BE A MINIMUM OF 0.5 THICK.
SU4-8 N 73+33 TO 81+74 (36' TO 42' RT) SU4-9 N 73+33 TO 81+74 (0' TO 6' LT)			560 560	190 190								2 2	3. BARRIER DELINEATOR PER STANDARD PLA M-40.10 SPACED AT 40'. RAISED PAVEMENT
SU5-1 N 79+94 TO 92+76 (28' RT) SU5-2 N 81+74 TO 92+76 (6' LT) SU5-3 N 81+74 TO 92+76 (42' RT) SU5-4 BRIDGE TRANSITION - SOUTH APPROACH			140	20		1457	56 56	0.6 0.6				4, 7 3,5 3,5	MARKER TYPE 2 SPACED AT 40'. 4. TEMPORARY BARRIER INSTALLED PER STANDARD PLANS K-80.35 WITH TYPE 1 ANCHORS
U5-5 BRIDGE TRANSITION - NORTH APPROACH U6-1 N 92+51 TO 94+51 (0' TO 6' LT) U6-2 N 92+51 TO 94+51 (36.5' TO 39.8' RT)			140 120 70	20 40 10								2 1	5. ADDITIONAL RAISED PAVEMENT MARKER TYPE 2 QUANTITIES LOCATED ON QTMK1.
U14-1 S 78+88 TO 101+11 (0') U14-2 S 78+08 TO 101+11 (12' LT)	2230 2310	0.6 0.6											6. ALL TEMPORARY PAVEMENT MARKINGS SHALL BE TAPE ACCORDING TO STANDARD SPECIFICATION 9-34.5(2).
J14-3 S 78+08 TO 101+11 (24' LT)	2310	0.6											
J14-4 S 78+88 TO 101+11 (36' LT) J14-5 S 78+88 TO 101+11 (0')	2230	0.6							2230			6	7. ROTOHAMMERING WILL BE REQUIRED TO INSTALL BARRIER ANCHOR PINS. AFTER ANCHORS ARE REMOVED, HOLES SHALL BE
J14-6 S 78+08 TO 101+11 (12' LT)									2310			6	FILLED WITH TYPE 1 EPOXY RESIN PER STD. SPEC. 9-26.1(1)A.
J14-7 S 78+08 TO 101+11 (24' LT)									2310			6	
U14-8 S 80+89 TO 92+71 (6' RT) U14-9 S 80+89 TO 92+71 (42' LT)							60 60	0.6				3,5 3,5	
014-10 S 78+88 TO 94+50 (28' LT) 014-11 BRIDGE TRANSITION - SOUTH APPROACH			200	30		1562		0.0				4, 7	
U15-1 S 97+94 TO 101+11 (0.5' TO 3.8' RT)			120	20								1	
U15-2 S 92+71 TO 101+11 (36' TO 42' LT)			560	190								2	
SHEET TOTAL	17720	4.8	2470	710	OTATE	3019	232	2.4	13370				_
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HECKED BY A. ROMANCHOCK S. FELL C. FRANCIS DATE DATE				22>	UMBER (333 ACT NO.	NHFP-0	051(316)			Washington St Department of	Transportation		QUANTITY TABULATION - STAGING

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QUANTITITIADULATION - STAGING				þ			1												GENERAL NOTES:	
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NOTES:

DETAILS.

ENGINEER.

SHOULDERS, AND RAMPS.

DRUMS OPTIONAL

WITHIN 5+/- MINUTES

WATCH FOR SLOW TRAFFIC

PROVISION.

1 THIS PLAN IS USED IN CONJUNCTION WITH APPLICABLE 2-LANE FREEWAY

2. SEE SMART WORK ZONE SYSTEM (SWZS) SPECIAL PROVISION OR RFP FOR

"##" ARE CHANGEABLE VALUES BASED ON REAL-TIME TRAVEL DELAY TIMES.

3. MODIFICATIONS TO PCMS MESSAGES SHALL BE ACCEPTED BY THE

4. ADJUST SWZS COMPONENTS TO AVOID CONFLICTS WITH SEQUENTIAL

5. LOCATE PCMSs PER STANDARD SPECIFICATION 1-10.3(3)C. PCMS MAY BE PLACED ON OPPOSITE SHOULDER BUT AVOID RAMP GORES. WHEN LOCATED BEHIND BARRIER/GUARDRAIL OR WITHIN CLOSURE, TRANSVERSE TRAFFIC

6. MINITURE PCMS (~6'WIDE, 12+ INCH CHARACTERS) ALLOWED FOR PCMS1. 7. IN LIEU OF TRAVEL TIME READERS, ALTERNATIVE METHODS (SUCH AS

9. IF TRAFFIC QUEUES REACH 8 MILES, PLACE ADDITIONAL PCMS AT 9.5 MILES RELOCATE TO REMAIN 0.5+/- MILE IN ADVANCE OF

QUEUE. TRUCK-MOUNTED PCMS WITH 10+ INCH CHARACTERS

ACCEPTABLE. TRANSVERSE TRAFFIC SAFETY DRUMS OPTIONAL.

PCMS MESSAGE: TRAFFIC BACKUPS PRESENT /

10. AVOID PCMS PLACEMENT ON HORIZONTAL CURVES.

REMOVE PCMS WHEN DISSIPATING QUEUES ARE LESS THAN 8

ARROW SIGNS OR OTHER TRAFFIC CONTROL DEVICES, NARROW

SINGLE RIGHT LANE CLOSURE TRAFFIC CONTROL PLAN (WITH PCMSs IN

ADVANCE OF LANE CLOSURE TAPER REMOVED)

MAXIMUM CHANNELIZATION DEVICE SPACING (feet) TAPER TANGENT MPH 40 80 50-75 35-45 30 60

20 40 20-30

DEVISE THEIR OWN STAGING PLANS PRIOR APPROVAL IS REQUIRED FOR NEW STAGING PLANS, OR FOR 36" MODIFICATIONS TO THIS ONE. SPEED THE CONTRACTOR SHALL HAVE AN ADDITIONAL 3 PCMS LIMIT BOARDS FOR ADVANCE NOTIFICATION ON LOCAL ROADS 60 DURING EACH EXTENDED CLOSURE PLACEMENT OF THE PCMS BOARDS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER. W3-5 R2-1 (B/W) GATED W3-5 GATED R2-1 (B/W) MP 18.88 0.1 +/-MILE 0.1 +/-MILE 0.1 +/-MILE 0.1 +/-MILE MILE MILE -----NB I-5 (BRIDGE #05/40E) --GATED W20-1 MP 18.20 GATED W20-5R MP 18.69 SEE NOTE 7 SWZS PCMS 8 SWZS PCMS 3 SWZS PCMS 2 SWZS PCMS 7 SWZS PCMS 6 SWZS PCMS 5 SWZS PCMS 4 WORK 9-MILE SMART WORK ZONE SYSTEM AND SIGNAGE FOR NB LEWIS BRIDGE (#05/40E) **AHEAD** AHFAD

USING TRAFFIC SENSOR SPEED DATA) IS ACCEPTABLE WHEN ACCURATE 8. IF SYSTEM FAILS SEE "SMART WORK ZONE SYSTEM FAILURE PROTOCOL" TRIGGEF SPEED W20-1 W20-5R SYMBOL CONDITION 35+ Free Flow <35 Slowed QUEUE PCMS 8 **PCMS** PCMS 6 PCMS 5 PCMS 4 PCMS **PCMS** TRAFFIC SENSORS LOCATION H G F E D C B A (miles) TRAFFIC CONDITION 2.0 SEC 2.0 SEC FF|FF|FF|FF|FF|FF (Blank) (Blank) (Blank) (Blank) LANE MILE (Blank) CLOSED AHFAD SINGLE TRAFFIC NEXT LOW OR |FF|FF|FF|FF|SL 0.5 TO 1.4 (Blank) (Blank) (Blank) (Blank) IANE MILES BACKLIPS MINITE STOPPED (Blank) CLOSURE AHFAD PRESENT DFI AY TRAFFIC MILE SINGLE TRAFFIC SLOW OR NEXT ZIDDER USE 7IPPFR TAKE 1.41 TO 2.4|FF|FF|FF|FF|SL|SL (Blank) (Blank) (Blank) LANE MILES BACKUPS MINUTE STOPPED MERGE RIGHT MERGE TURNS MILES PRESENT CLOSURE AHEAD DELAY TRAFFIC AHEAD ANE TO HERE SINGLE TRAFFIC SLOW OF NEXT 2 MILES USE 7IPPFR USF 7IPPFR TAKE ## 2.41 TO 3.4 FF FF FF FF SL SL SL (Blank) MILES BACKUPS MINUTE STOPPED MERGE RIGHT MERGE (Blank) LANE O MERGE ALL TURNS CLOSURE AHEAD PRESENT DELAY TRAFFIC MILES POINT 3 LANES AHEAD ANE TOO HERE SINGLE 3 MILES TAKE TRAFFIC SLOW OR NEXT USE 2 MILES USE ZIPPER USE ZIPPER 3.41 TO 4.9 FF FF FF SLISLISLISL LANE MILES BACKUPS MINUTE O MERGE O MERGE MERGE RIGHT MERGE TURNS (Blank) STOPPED ALL ALL LOSURE AHEAD PRESENT **TRAFFIC** MILES POINT POINT 3 LANES AHEAD LANE TOO HERE DELAY LANES SINGLE TRAFFIC SLOW OR 4.5 MILES 3 MILES USE 2 MILES ZIPPER ZIPPER TAKE ## NEXT USE USE USE 4.91 TO 6.4|FF|FF|SL|SL|SL|SL|SL LANE MILES BACKUPS MINUTE STOPPED O MERGE O MERGI O MERGE MFRGF RIGHT **MERGE** ALL ALL **TURNS** PRESENT MILES POINT LOSURE AHEAD DELAY **TRAFFIC** POINT POINT LANES LANES **AHEAD** ANE TO HERE SLOW OR 6 MILES 4.5 MILES USE 3 MILES 2 MILES ZIPPER USE ZIPPER TAKE 1 LANE NEXT USE USE USE FF|SL|SL|SL|SL|SL CLOSURE O MERGE O MERGE O MERGE O MERGE 6.41 TO 7. MINUTE STOPPED ALL MERGE RIGHT MERGE TURNS

ALL SIGNS ARE 48"x48" BLACK ON ORANGE UNLESS

SIGNS SHOWN WITHIN THESE STAGING PLANS SHALL BE

CONFLICTING SIGNS SHALL BE COVERED IN ACCORDANCE

WITH STANDARD SPEC. 8-21.3(3) (SIGN COVERING WILL BE

THESE STAGING PLANS ARE FOR GENERAL INFORMATION

THE PROJECT. WHERE NEEDED, THE CONTRACTOR SHALL

ONLY AND DO NOT SHOW EVERY STAGE OR CONSTRUCTION

ACTIVITY THAT THE CONTRACTOR MAY NEED TO COMPLETE

OTHERWISE INDICATED

CLASS A AND POST-MOUNTED

PAID UNDER OTHER ITEMS OF WORK).

FILE NAME	\\Wsdot.loc\sw\Group\EngIneering\444301\05-DesIgn\02-Projects\I-5 NB & SB LewIs R	Iver Brand N	NB To	utle River Br_i	Deck Repair\1-CADD-Plans_Coi	tractPlans\008-027_22X333_PS_S	UA.dgn			Plot 1
TIME	1:21:20 PM			REGION STATE	FED.AID PROJ.NO.	APPERE A			l-5	PLAN REF NO
DATE	3/16/2023			10 WASH	NHFP-0051(316)	OF WASH				SU1
PLOTTED BY	hillb			IU WASH	, , , , , , , , , , , , , , , , , , , ,				NB AND SB LEWIS RVR BR	
DESIGNED BY	T. PANKAU			JOB NUMBER		Vandilla Fond Dell		Washington State	DECK REPAIR / OVERLAY	SHEET
ENTERED BY	CCEO			22X333	/			3		9
CHECKED BY	A. ROMANCHOCK			CONTRACT NO.	LOCATION NO.	36748 PEGISTERES		Department of Transportation		OF
PROJ. ENGR.	S. FELL				XL6379	SEE SHEET CT1	DATE	-	STAGING PLAN - BRIDGE 5/40E STAGE A	84
REGIONAL ADM	C ERANCIS REVISION	DATE	RV		/ AE03/3	DE STAND DOX	DE STAND DOY			SHEETS

MILES

MINUTE

DELAY

POINT

6 MILES

TO MERGE

POINT

3 LANES

USE

ALL

3 LANES

POINT

1.5 MILES

POINT

O MERGE

3 LANES

USE

ALL

3 LANES

POINT

3 MILES

TO MERGE

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LANES

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ALL

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2 MILES

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ALL

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AHEAD

ZIPPER

MERGE

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ANE TO

USE

RIGHT

LANE TO

HERE

ZIPPER

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TAKE

TURNS

TRAFFIC

1 LANE

CLOSED

7.5 MILES

9 MILES

SLOW OF

STOPPED

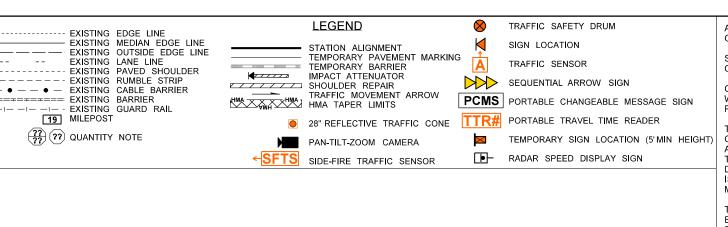
TRAFFIC

SL|SL|SL|SL|SL|SL

DELAY

NEXT

MILES



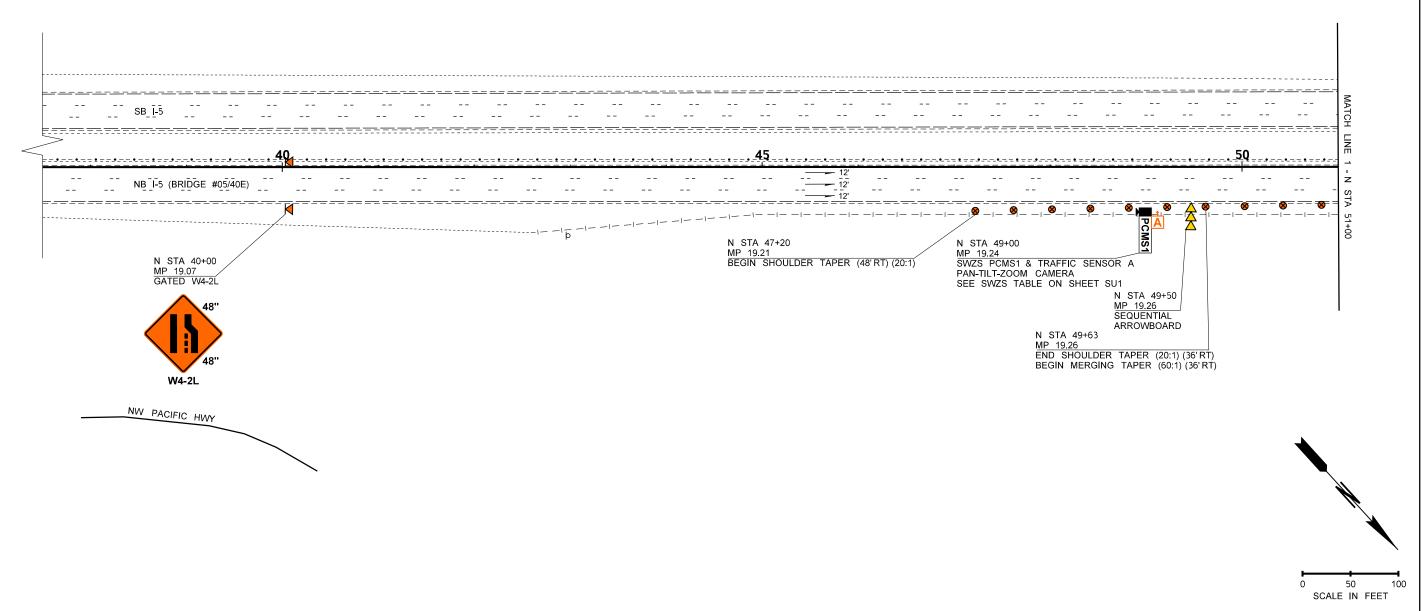
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CONFLICTING SIGNS SHALL BE COVERED IN ACCORDANCE WITH STANDARD SPEC. 8-21.3(3) (SIGN COVERING WILL BE PAID UNDER OTHER ITEMS OF WORK).

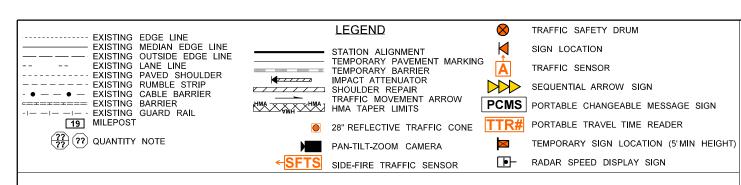
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MAXIMU DEVIC	M CHANNE E SPACING	LIZATION (feet)				
MPH	TAPER	TANGENT				
50-75	40	80				
35-45	30	60				
20-30	20	40				



FILE NAME	\\Wsdot.loc\sw\Group\Englnee	rlng\444301\05-DesIgn\02-Projects\I-5 NB & SB LewIs R	Iver Brand N	NB Toutle R	Iver Br_De	eck Repair\1-CADD-Plans_Co	rtractPlans\008-027_22X333_PS_S	UA.dgn			Plot 2
TIME	1:21:21 PM			REGION NO.	STATE	FED.AID PROJ.NO.	A PARAGE			I-5	PLAN REF NO
DATE	3/16/2023			10	WASH	NHFP-0051(316)	OF WASHIC				SU2
PLOTTED BY	hillb			10	WASH					NB AND SB LEWIS RVR BR	002
DESIGNED BY	T. PANKAU				NUMBER		Market Reon El		Washington State	DECK REPAIR / OVERLAY	SHEET
ENTERED BY	CCEO			22X	(333				J		10
CHECKED BY	A. ROMANCHOCK			CONT	RACT NO.	LOCATION NO.	36748 PEGISTERES		Department of Transportation		OF OF
PROJ. ENGR.	S. FELL					XL6379	SEE SHEET CT1	DATE		STAGING PLAN - BRIDGE 5/40E STAGE A	N 84
REGIONAL ADM.	. C. FRANCIS	REVISION	DATE	BY		7(20313	P.E. STAMP BOX	P.E. STAMP BOX			SHEETS



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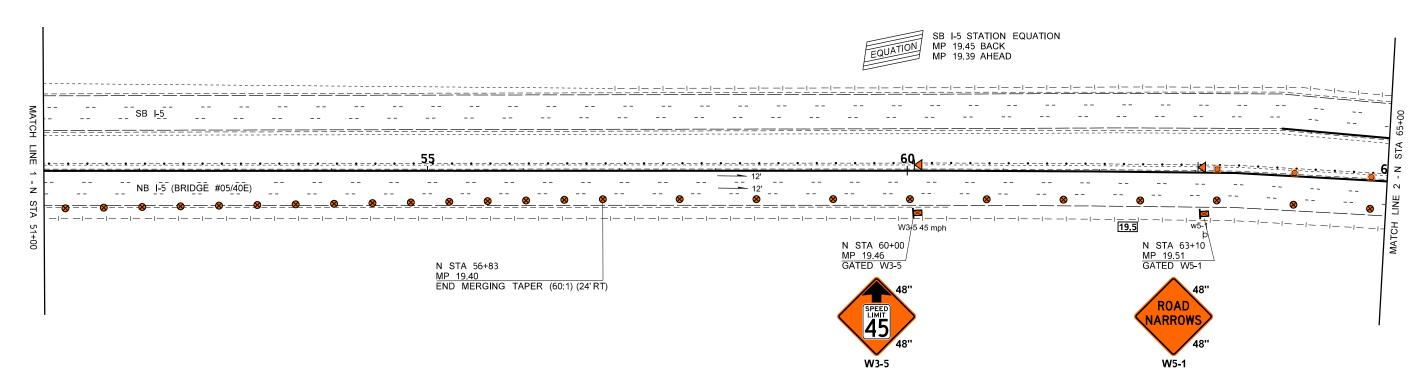
 MAXIMUM DEVICE
 CHANNELIZATION (feet)

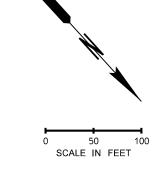
 MPH
 TAPER
 TANGENT

 50-75
 40
 80

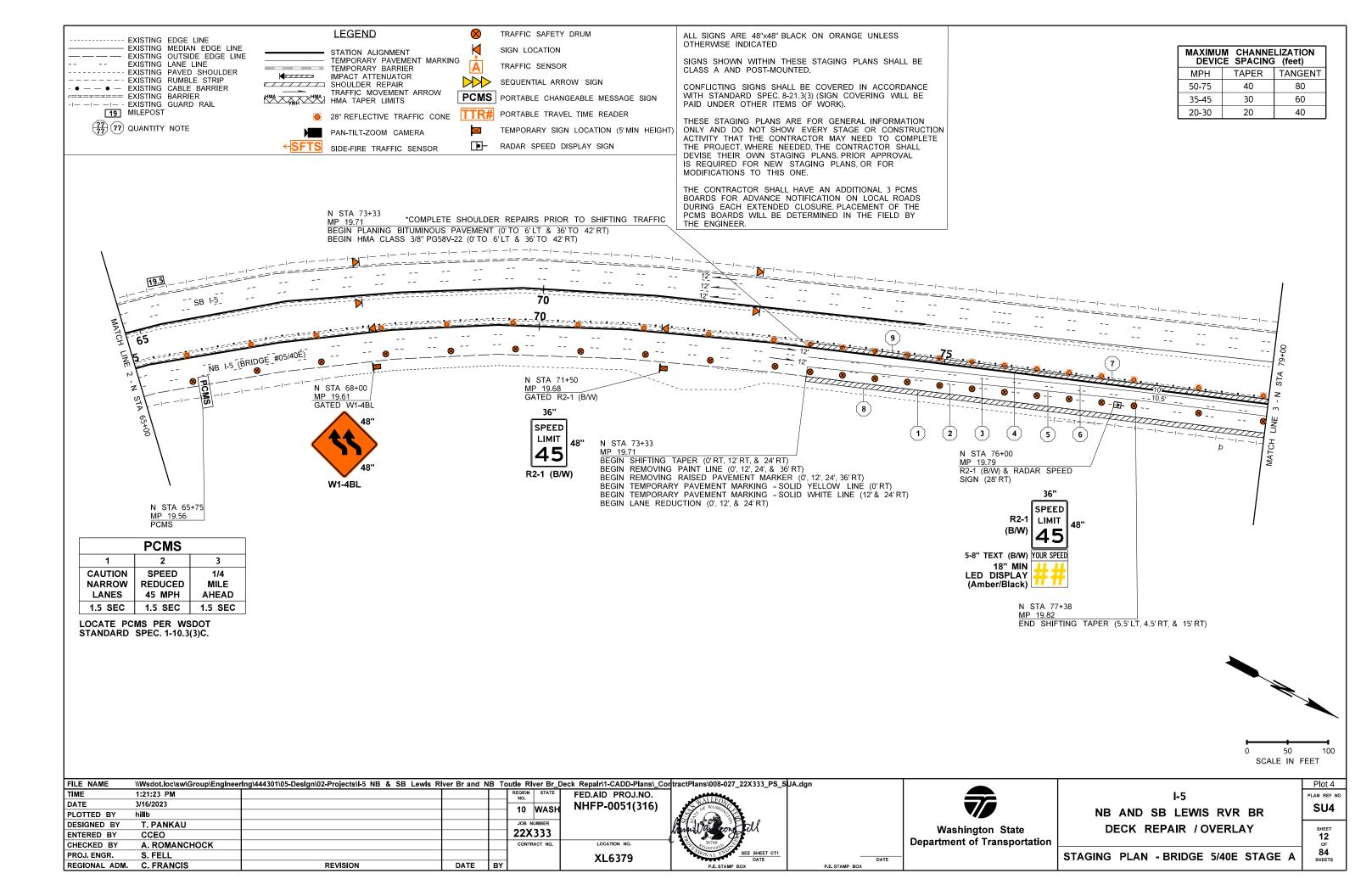
 35-45
 30
 60

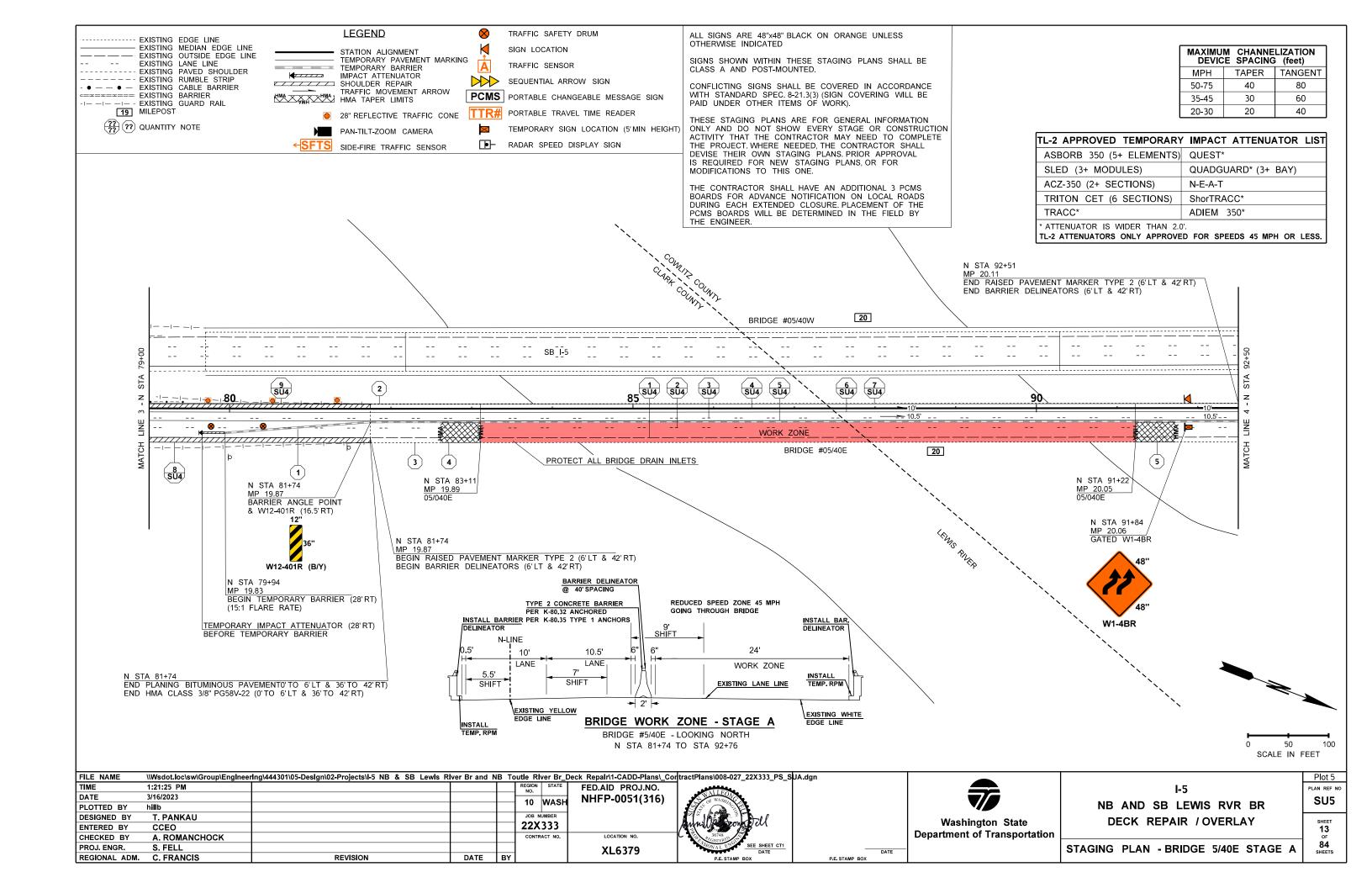
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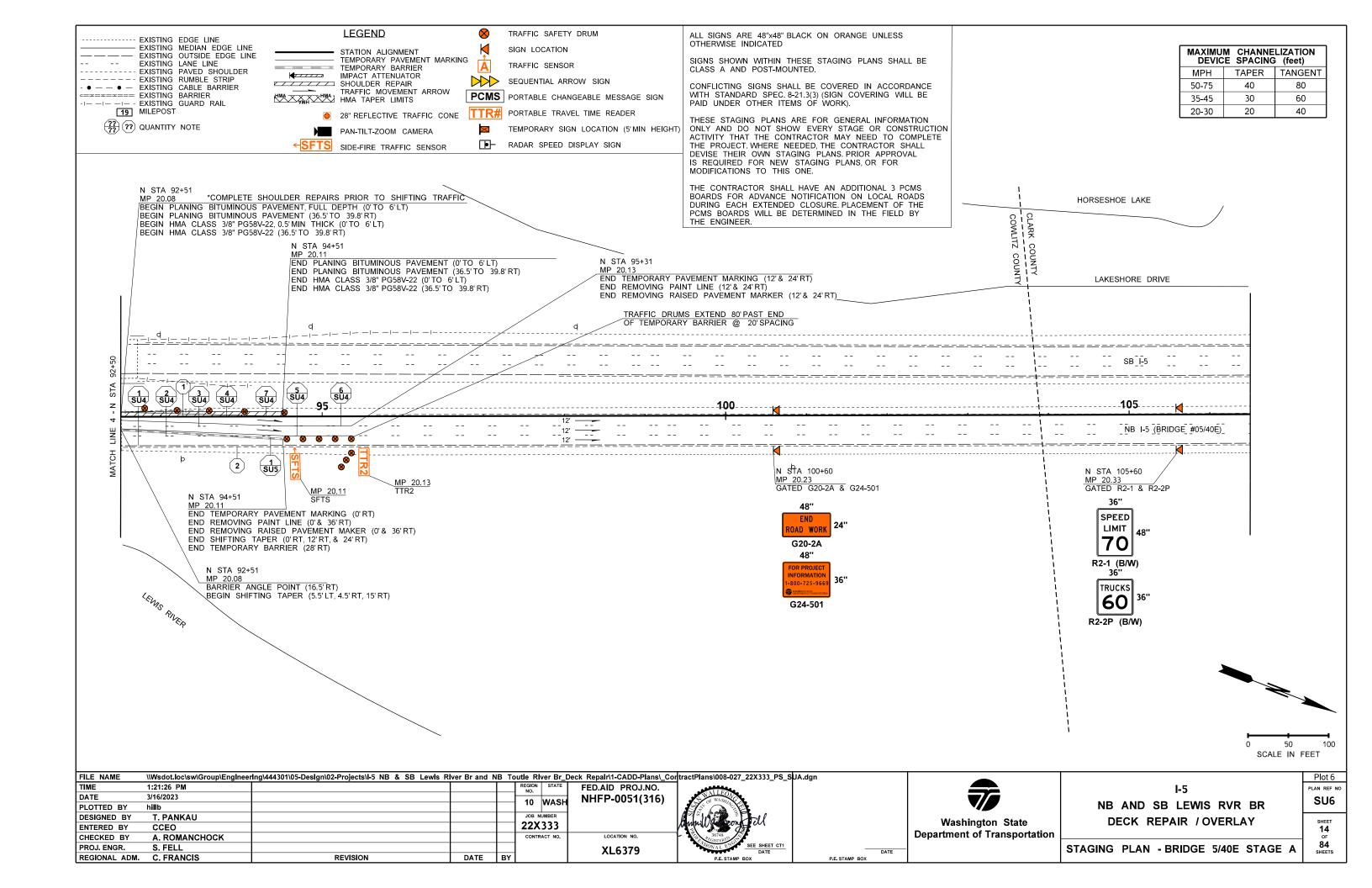


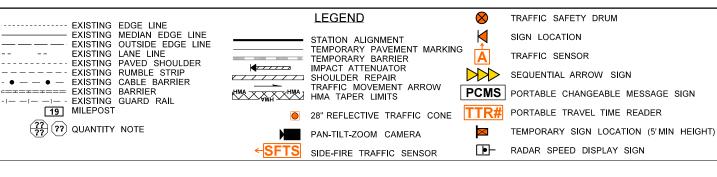


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PLOTTED BY	hillb				IU WASH					NB AND SB LEWIS RVR BR	
DESIGNED BY	T. PANKAU				JOB NUMBER				Washington State	DECK REPAIR / OVERLAY	SHEET
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PROJ. ENGR.	S. FELL					XL6379	SIONAL EN SEE SHEET CT1 DATE	DATE		STAGING PLAN - BRIDGE 5/40E STAGE A	84 SHEETS
REGIONAL ADM.	C. FRANCIS	REVISION	DATE	BY		Λ203.3	P.E. STAMP BOX	P.E. STAMP BOX			0









MAXIMUM CHANNELIZATION **DEVICE SPACING (feet)** TAPER TANGENT MPH 40 80 50-75 35-45 30 60 20 40 20-30

ACTIVITY THAT THE CONTRACTOR MAY NEED TO COMPLETE THE PROJECT. WHERE NEEDED, THE CONTRACTOR SHALL DEVISE THEIR OWN STAGING PLANS PRIOR APPROVAL 36" SPEED LIMIT 60 W3-5 R2-1 (B/W) GATED R2-1 (B/W) GATED W3-5 N 39+50 MP 19 07 MP 1869 0.1 +/-MILE MILE - -GATED W20-1 STA 9+50 GATED W20-5L N 29+50 SWZS PCMS 3 SWZS PCMS 2 MP 18 88 MP 18 50 17.25 48' WORK

IS REQUIRED FOR NEW STAGING PLANS, OR FOR MODIFICATIONS TO THIS ONE. THE CONTRACTOR SHALL HAVE AN ADDITIONAL 3 PCMS BOARDS FOR ADVANCE NOTIFICATION ON LOCAL ROADS DURING EACH EXTENDED CLOSURE PLACEMENT OF THE PCMS BOARDS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER. 0.1 +/-MILE ► 0.1 +/-MILE 0.1 +/-MILE MILE _____ NB I-5 (BRIDGE #05/40E) --SEE NOTE 7 SWZS PCMS 8 SWZS PCMS 7 SWZS PCMS 6 SWZS PCMS 5 SWZS PCMS 4 CLOSED 9-MILE SMART WORK ZONE SYSTEM AND SIGNAGE FOR NB LEWIS BRIDGE (#05/40E) AHEAD AHFAD

TRIGGEF SPEED W20-1 SYMBOL W20-5L CONDITION 35+ Free Flow <35 Slowed QUEUE PCMS 8 **PCMS** PCMS 6 PCMS 5 PCMS 4 PCMS **PCMS** TRAFFIC SENSORS LOCATION H G F E D C B A (miles) TRAFFIC CONDITION 2.0 SEC 2.0 SEC FF|FF|FF|FF|FF|FF (Blank) (Blank) (Blank) (Blank) LANE MILE (Blank) CLOSED AHFAD SINGLE TRAFFIC NEXT LOW OR |FF|FF|FF|FF|SL 0.5 TO 1.4 (Blank) (Blank) (Blank) (Blank) IANE MILES BACKLIPS MINITE STOPPED (Blank) CLOSURE AHFAD PRESENT DFI AY TRAFFIC MILE SINGLE TRAFFIC SLOW OR NEXT ZIDDER USE 7IPPFR TAKE 1.41 TO 2.4|FF|FF|FF|FF|FF|SL|SL (Blank) (Blank) (Blank) LANE MILES BACKUPS MINUTE STOPPED MERGE LEFT MERGE TURNS MILES PRESENT CLOSURE AHEAD DELAY TRAFFIC AHEAD ANE TO HERE SINGLE TRAFFIC SLOW OF NEXT 2 MILES USE 7IPPFR USF 7IPPFR TAKE ## 2.41 TO 3.4 FF FF FF FF SL SL SL (Blank) MILES BACKUPS MINUTE STOPPED MERGE LEFT MERGE (Blank) LANE O MERGE ALL TURNS CLOSURE AHEAD PRESENT DELAY TRAFFIC MILES POINT 3 LANES AHEAD ANE TO HERE SINGLE 3 MILES TAKE TRAFFIC SLOW OR NEXT USE 2 MILES USE ZIPPER USE ZIPPER 3.41 TO 4.9 FF FF FF SLISLISLISL LANE MILES BACKUPS MINUTE O MERGE MERGE LEFT MERGE TURNS (Blank) STOPPED ALL O MERGE ALL LOSURE AHEAD PRESENT **TRAFFIC** MILES POINT POINT 3 LANES AHEAD LANE TO HERE DELAY LANES SINGLE TRAFFIC SLOW OR 4.5 MILES 3 MILES USE 2 MILES ZIPPER ZIPPER TAKE ## NEXT USE USE USE 4.91 TO 6.4 FF FF SL SL SL SL SL LANE MILES BACKUPS MINUTE STOPPED O MERGE O MERGI O MERGE MFRGF LEFT MERGE ALL ALL **TURNS** PRESENT MILES POINT LOSURE AHEAD DELAY **TRAFFIC** POINT LANE POINT LANES LANES **AHEAD** ANE HERE SLOW OR 6 MILES 4.5 MILES USE 3 MILES 2 MILES ZIPPER USE ZIPPER TAKE 1 LANE NEXT USE USE USE FF|SL|SL|SL|SL|SL CLOSURE O MERGE O MERGE O MERGE O MERGE LEFT 6.41 TO 7.9 MINUTE STOPPED ALL MERGE MERGE TURNS 9 MILES TRAFFIC MILES POINT 3 LANES POINT 3 LANES POINT POINT 3 LANES AHEAD ANE TO HERE DELAY LANES SLOW OF NEXT 1 LANE 6 MILES USE 4.5 MILES USE 3 MILES USE 2 MILES USE ZIPPER USE ZIPPER TAKE O MERGE SL|SL|SL|SL|SL|SL STOPPED CLOSED MINUTE TO MERGE ALL O MERGE ALL TO MERGE ALL ALL MERGE LEFT MERGE TURNS TRAFFIC MILES 7.5 MILES DELAY POINT 3 LANES POINT 3 LANES POINT 3 LANES POINT 3 LANES AHEAD LANE TO HERE

ALL SIGNS ARE 48"x48" BLACK ON ORANGE UNLESS

SIGNS SHOWN WITHIN THESE STAGING PLANS SHALL BE

CONFLICTING SIGNS SHALL BE COVERED IN ACCORDANCE

WITH STANDARD SPEC. 8-21.3(3) (SIGN COVERING WILL BE

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CLASS A AND POST-MOUNTED

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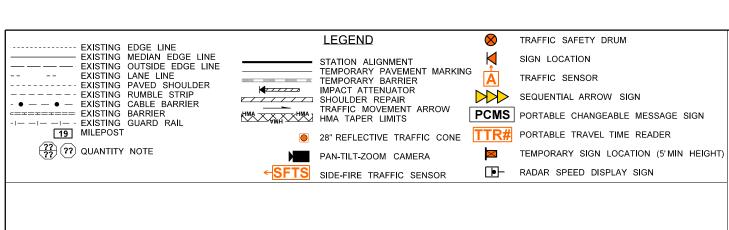
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CHECKED BY	A. ROMANCHOCK				CONTRACT NO.	LOCATION NO.	36748 PEGISTERED		Department of Transportation		OF
PROJ. ENGR.	S. FELL				,	XL6379	SIONAL ENGINEER SHEET CT1	DATE		STAGING PLAN - BRIDGE 5/40E STAGE B	84 SHEETS
REGIONAL ADM.	C. FRANCIS	REVISION	DATE	BY	,	1 7.20373	P.E. STAMP BOX	P.E. STAMP BOX			5.12215

NOTES:

- 1. THIS PLAN IS USED IN CONJUNCTION WITH APPLICABLE 2-LANE FREEWAY SINGLE RIGHT LANE CLOSURE TRAFFIC CONTROL PLAN (WITH PCMSs IN ADVANCE OF LANE CLOSURE TAPER REMOVED).
- 2. SEE SMART WORK ZONE SYSTEM (SWZS) SPECIAL PROVISION OR RFP FOR
- 3. MODIFICATIONS TO PCMS MESSAGES SHALL BE ACCEPTED BY THE ENGINEER.
- "##" ARE CHANGEABLE VALUES BASED ON REAL-TIME TRAVEL DELAY TIMES.
- 4. ADJUST SWZS COMPONENTS TO AVOID CONFLICTS WITH SEQUENTIAL ARROW SIGNS OR OTHER TRAFFIC CONTROL DEVICES, NARROW SHOULDERS AND RAMPS
- 5. LOCATE PCMSs PER STANDARD SPECIFICATION 1-10.3(3)C. PCMS MAY BE PLACED ON OPPOSITE SHOULDER BUT AVOID RAMP GORES. WHEN LOCATED BEHIND BARRIER/GUARDRAIL OR WITHIN CLOSURE, TRANSVERSE TRAFFIC DRUMS OPTIONAL.
- 6. MINITURE PCMS (~6'WIDE, 12+ INCH CHARACTERS) ALLOWED FOR PCMS1.
- 7. IN LIEU OF TRAVEL TIME READERS, ALTERNATIVE METHODS (SUCH AS USING TRAFFIC SENSOR SPEED DATA) IS ACCEPTABLE WHEN ACCURATE
- 8. IF SYSTEM FAILS SEE "SMART WORK ZONE SYSTEM FAILURE PROTOCOL" PROVISION.
- 9. IF TRAFFIC QUEUES REACH 8 MILES, PLACE ADDITIONAL PCMS AT 9.5 MILES, RELOCATE TO REMAIN 0.5+/- MILE IN ADVANCE OF QUEUE, TRUCK-MOUNTED PCMS WITH 10+ INCH CHARACTERS ACCEPTABLE TRANSVERSE TRAFFIC SAFETY DRUMS OPTIONAL REMOVE PCMS WHEN DISSIPATING QUEUES ARE LESS THAN 8 MILES

PCMS MESSAGE: TRAFFIC BACKUPS PRESENT / WATCH FOR SLOW TRAFFIC

10. AVOID PCMS PLACEMENT ON HORIZONTAL CURVES.



NW PACIFIC HWY

ALL SIGNS ARE 48"x48" BLACK ON ORANGE UNLESS OTHERWISE INDICATED

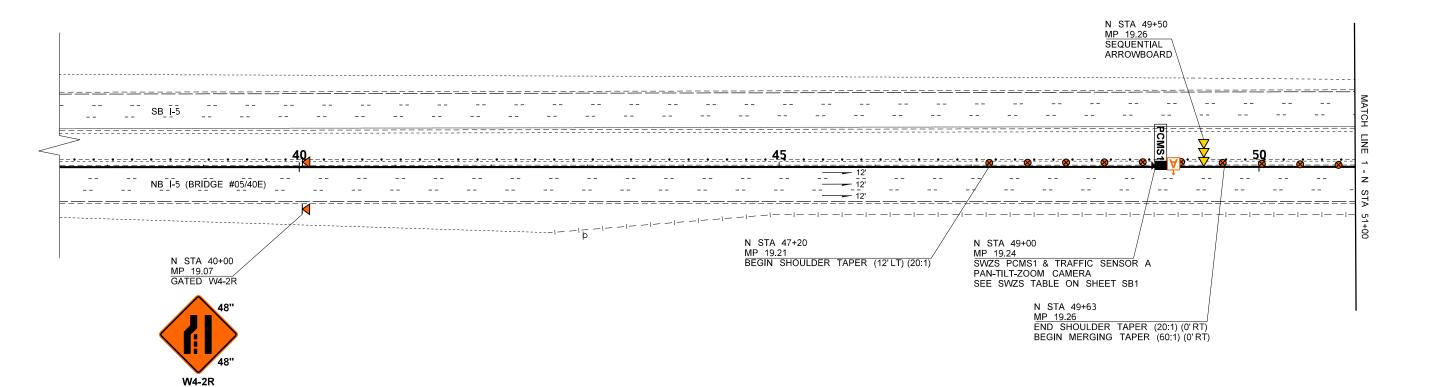
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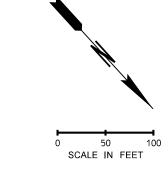
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MAXIMU DEVIC	M CHANNE E SPACING	LIZATION (feet)
MPH	TAPER	TANGENT
50-75	40	80
35-45	30	60
20-30	20	40





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DATE	3/16/2023				10 WASH	NHFP-0051(316)	OF WASHING				SU8
PLOTTED BY	hillb				IU WASH	,				NB AND SB LEWIS RVR BR	
DESIGNED BY	T. PANKAU				JOB NUMBER		June Na Soma Dell		Washington State	DECK REPAIR / OVERLAY	SHEET
ENTERED BY	CCEO				22X333				J		16
CHECKED BY	A. ROMANCHOCK				CONTRACT NO.	LOCATION NO.	36748 EGISTERED		Department of Transportation		OF OF
PROJ. ENGR.	S. FELL					XL6379	SEE SHEET CT1	——————————————————————————————————————		STAGING PLAN - BRIDGE 5/40E STAGE B	84 SHEETS
REGIONAL ADM.	C. FRANCIS	REVISION	DATE	BY		ALOSI 3	P.E. STAMP BOX	P.E. STAMP BOX			SILETS



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 MAXIMUM DEVICE
 CHANNELIZATION SPACING (feet)

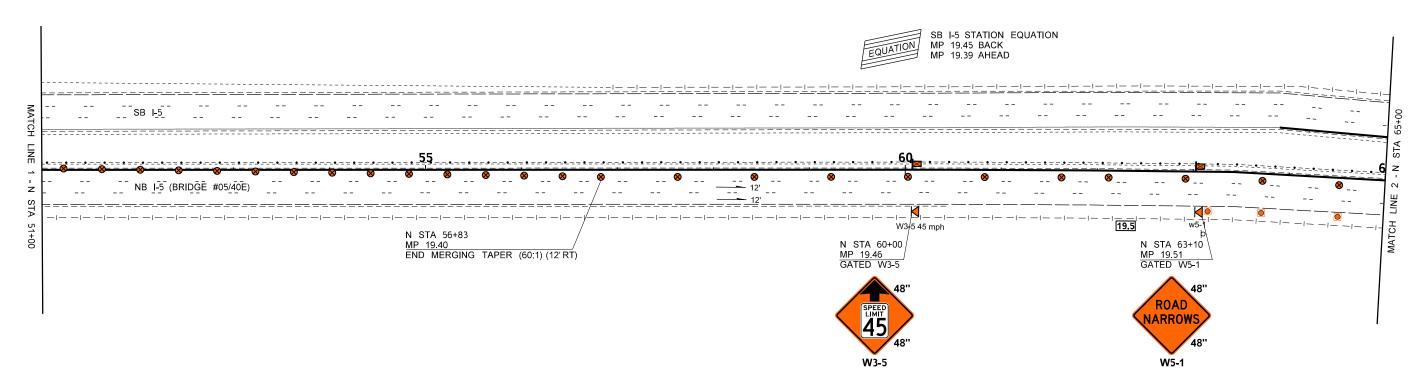
 MPH
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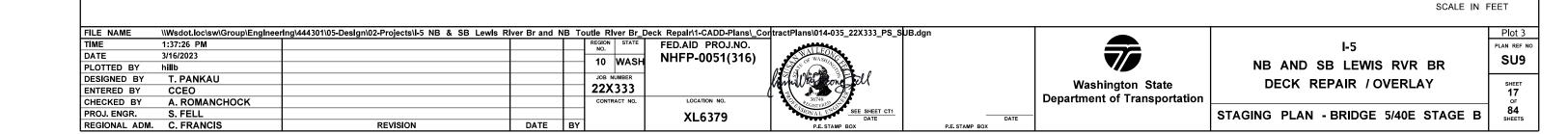
 50-75
 40
 80

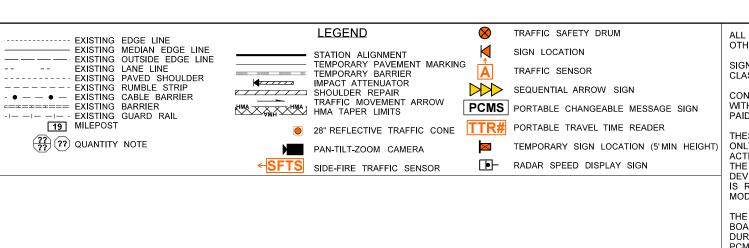
 35-45
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 20
 40

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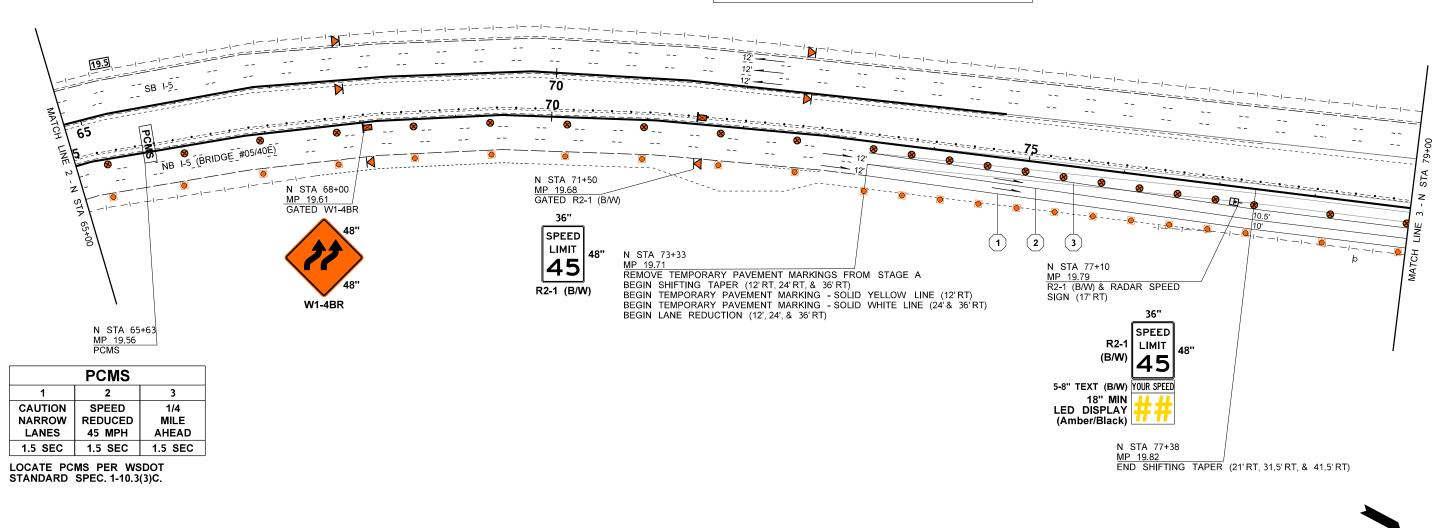
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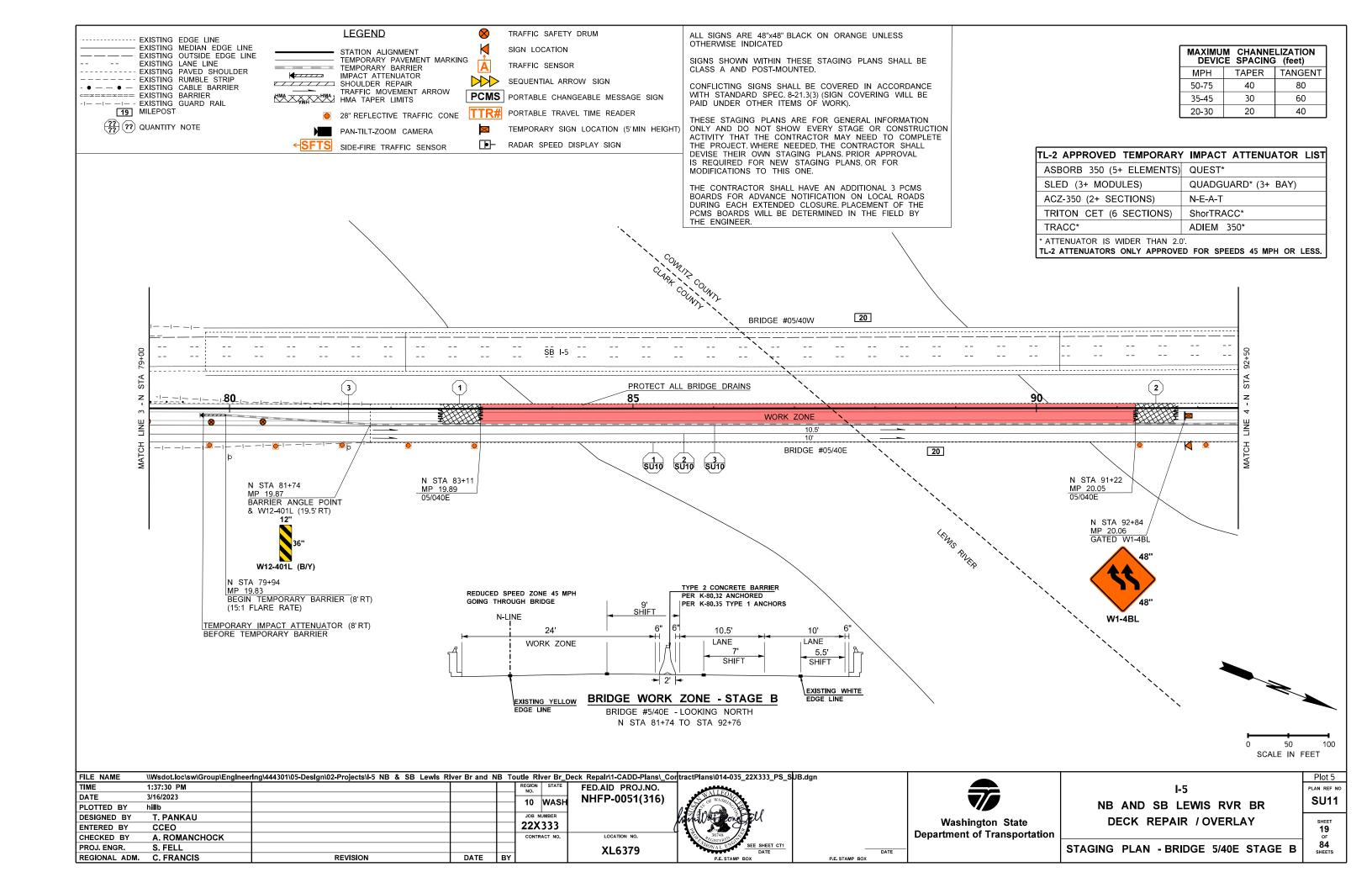
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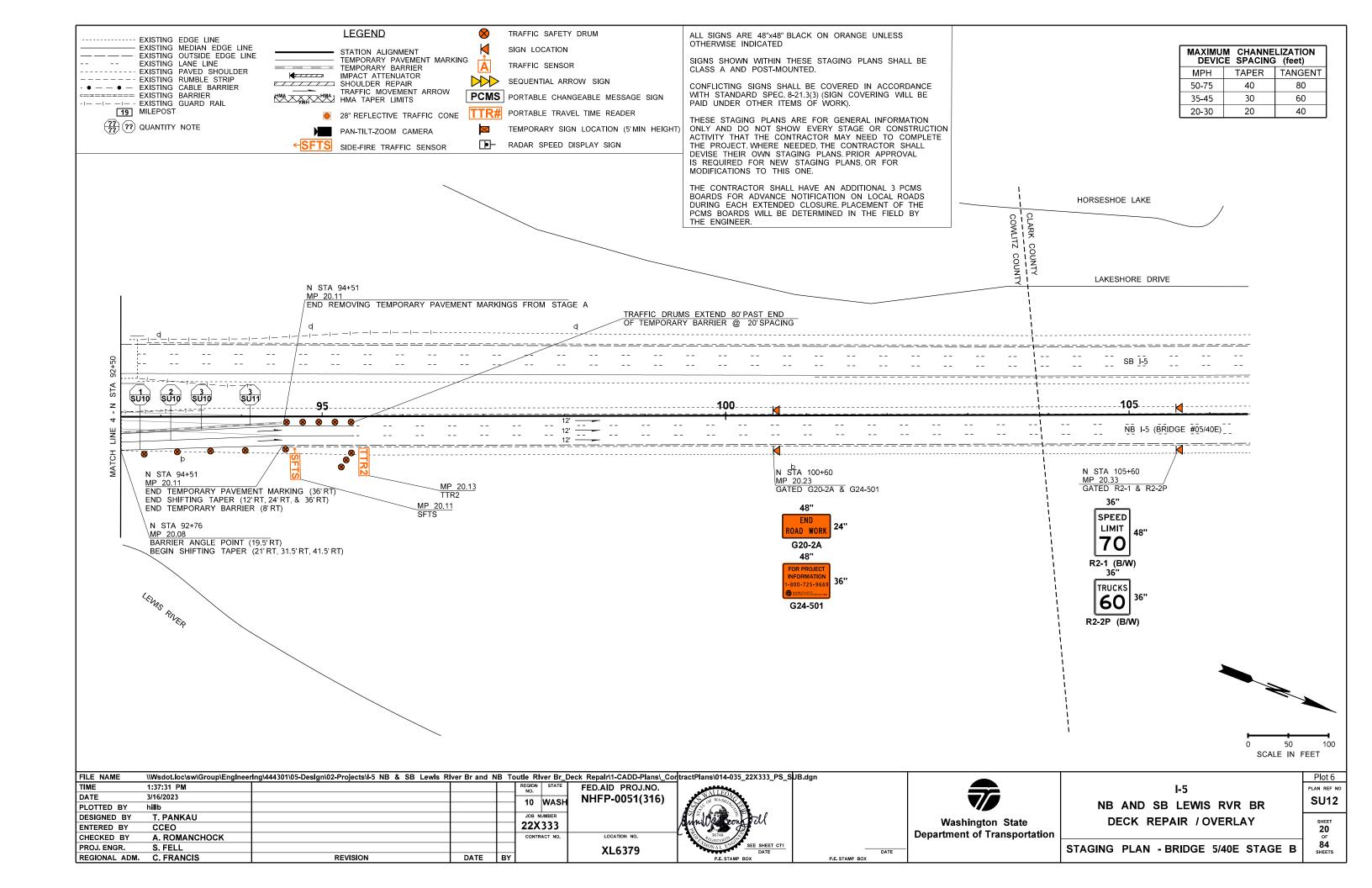
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MPH	TAPER	TANGENT		
50-75	40	80		
35-45	30	60		
20-30	20	40		

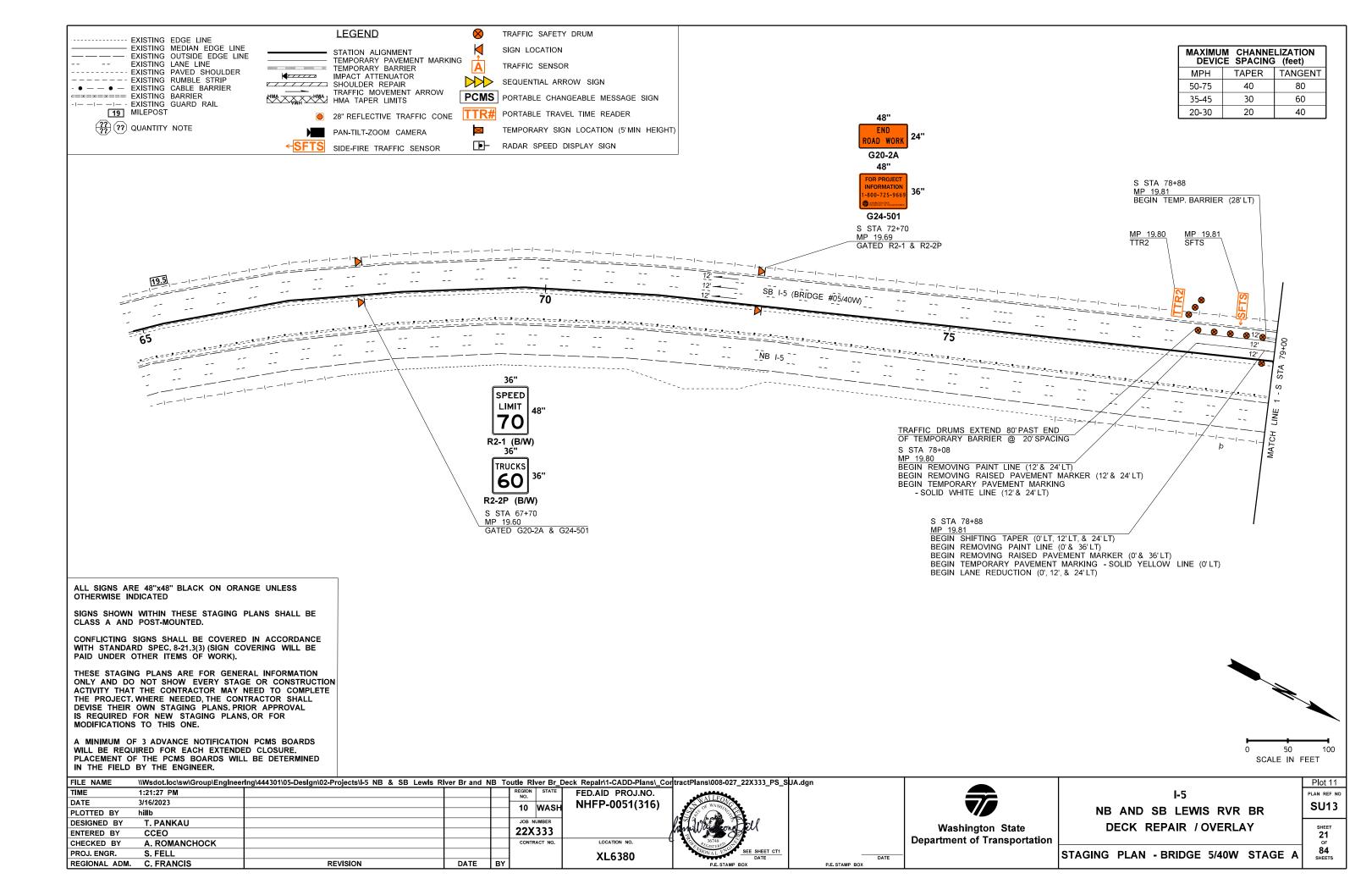
50 SCALE IN FEET

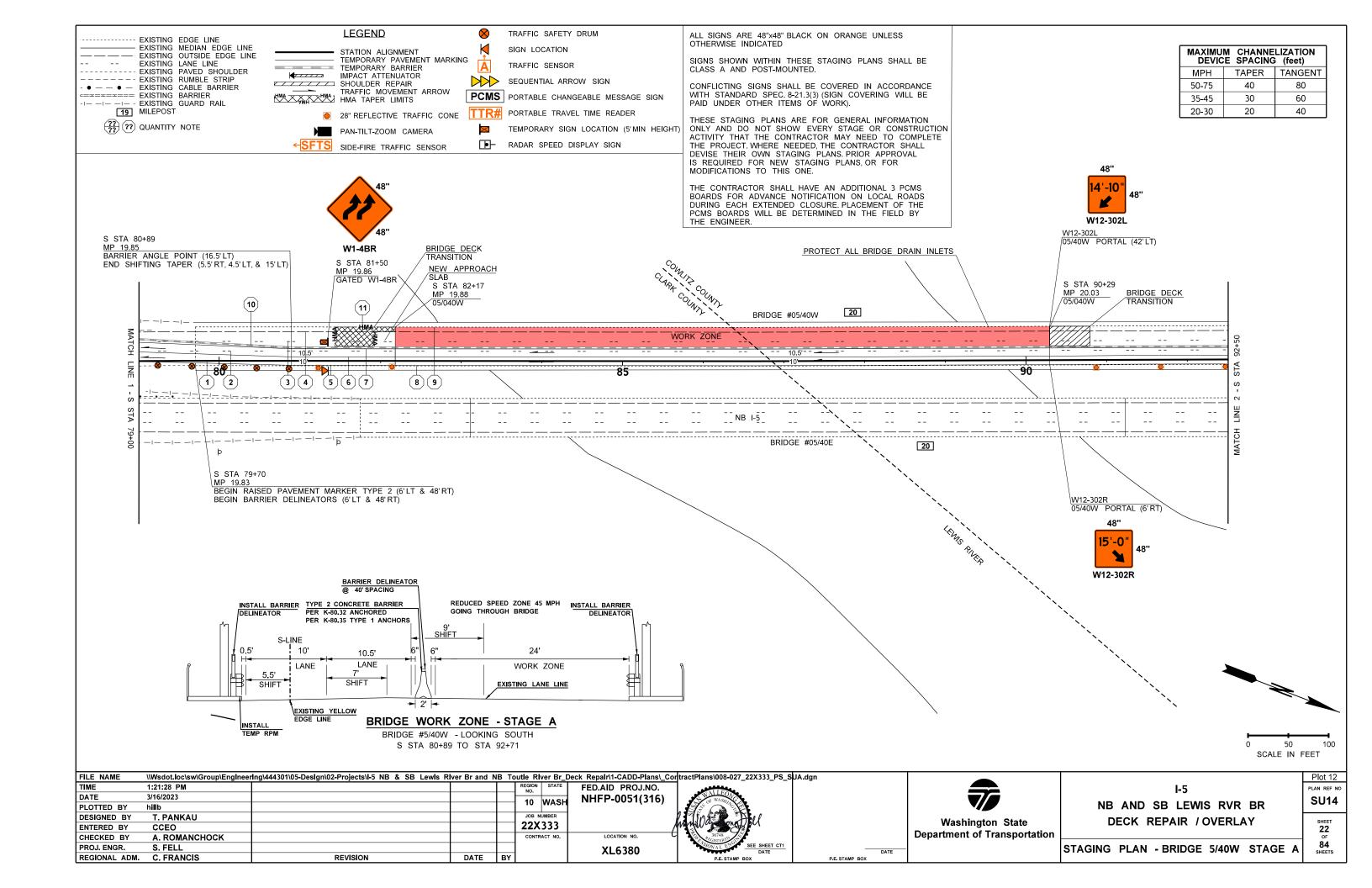


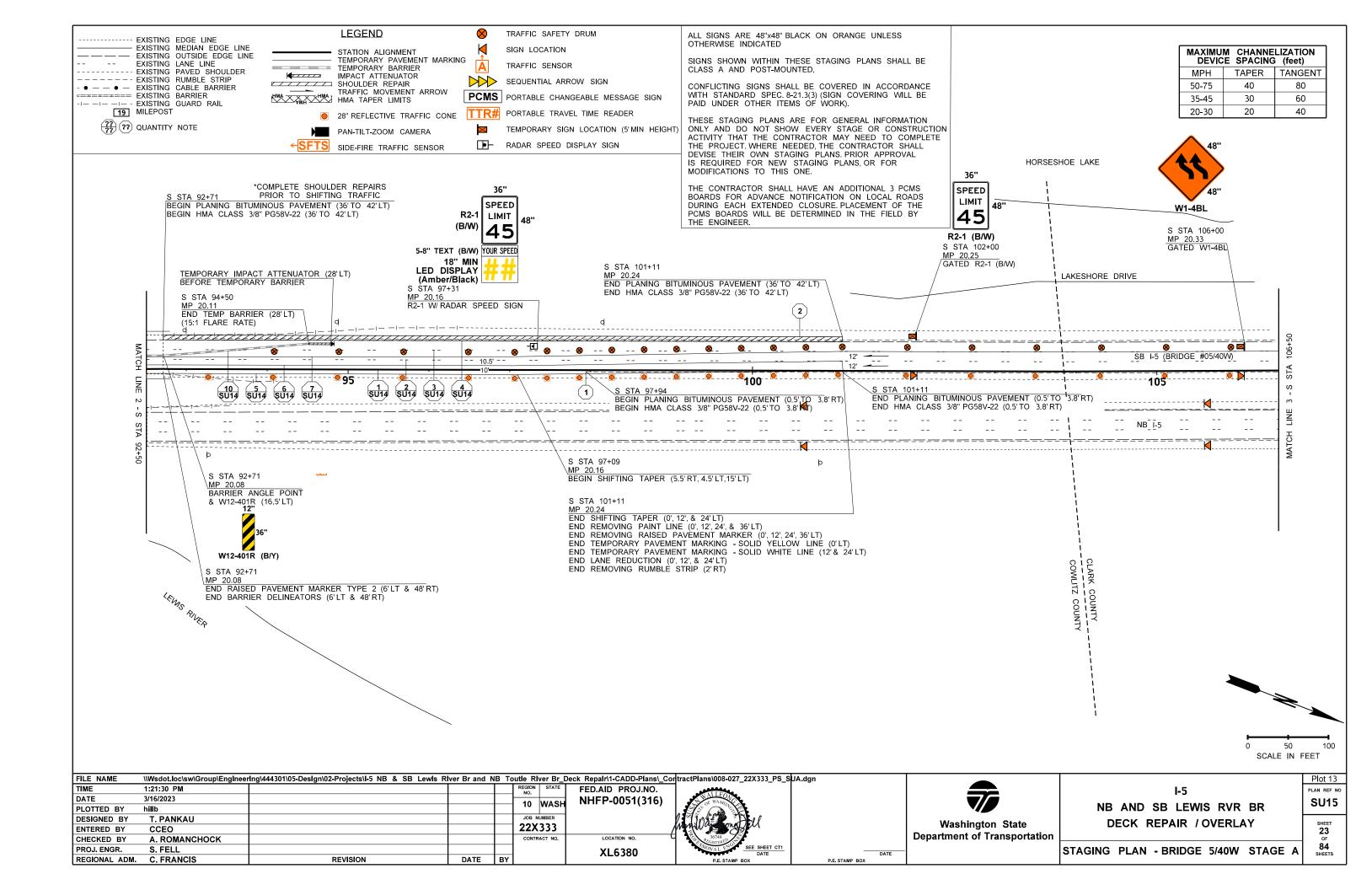
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DESIGNED BY	T. PANKAU			JOB NUMBER		Man Day Bon Fill		Washington State	DECK REPAIR / OVERLAY	SHEET
ENTERED BY	CCEO			22X333				J		18
CHECKED BY	A. ROMANCHOCK			CONTRACT NO.	LOCATION NO.	36748 PEGISTERED		Department of Transportation		_ OF
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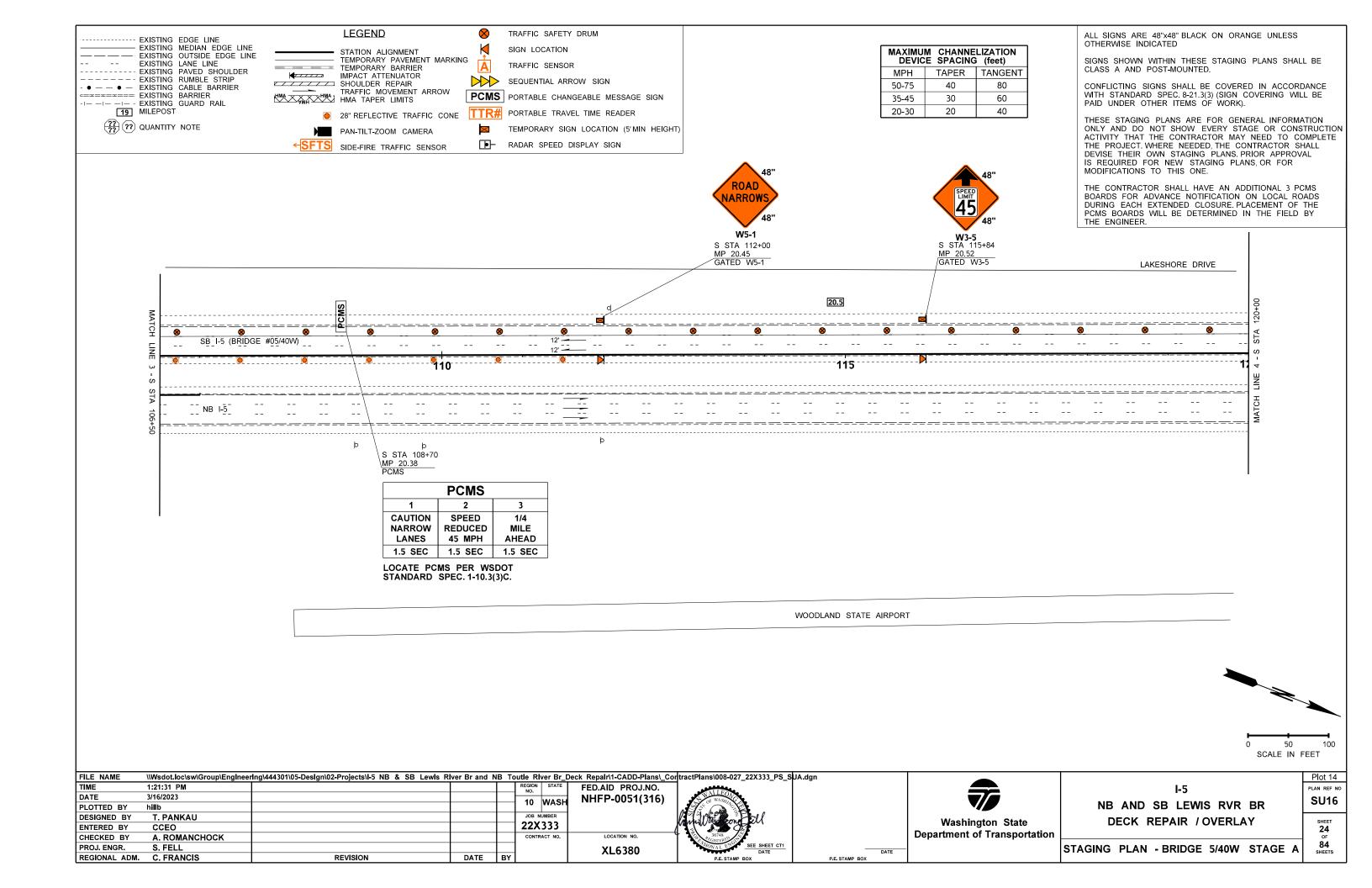


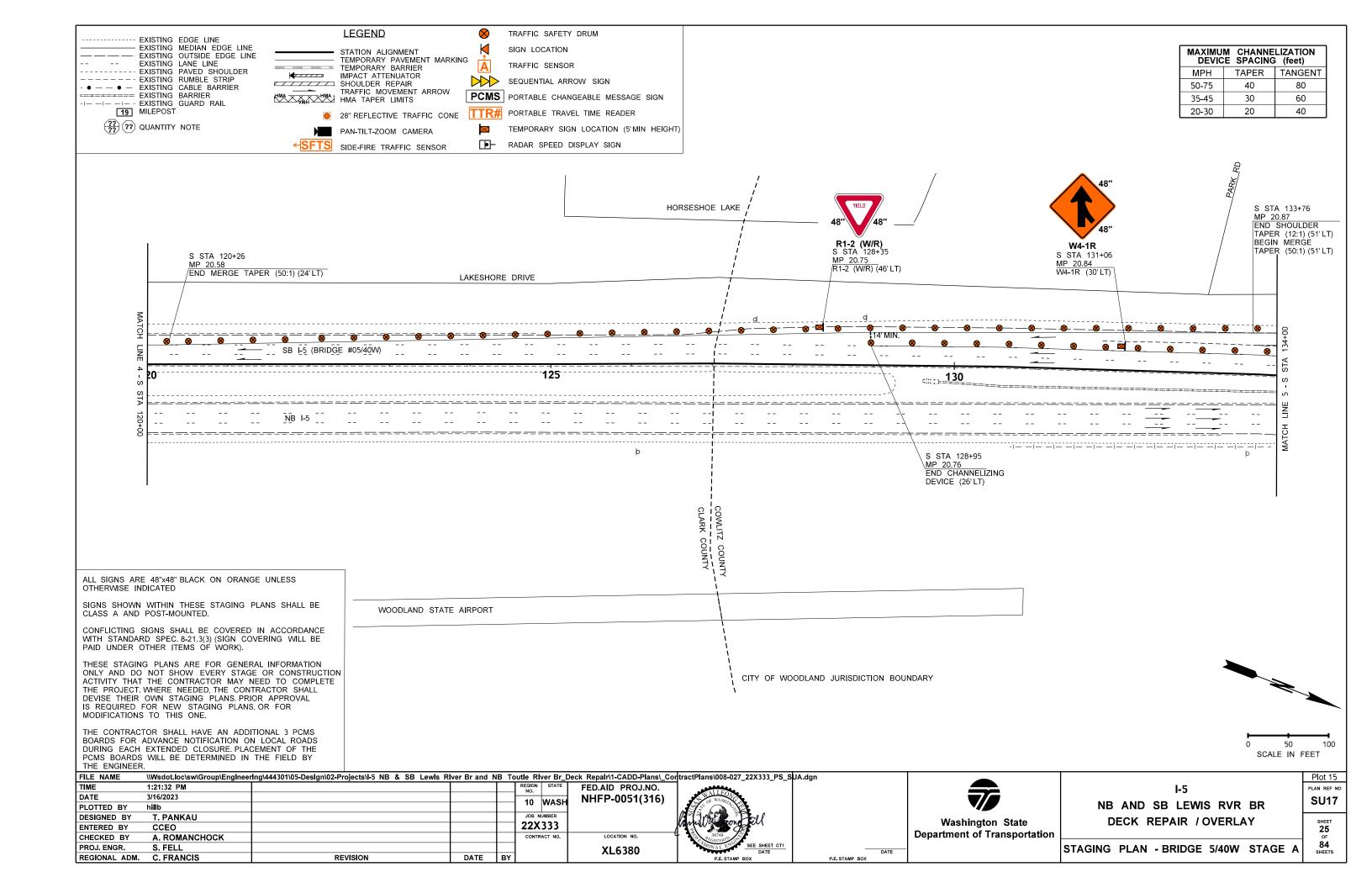


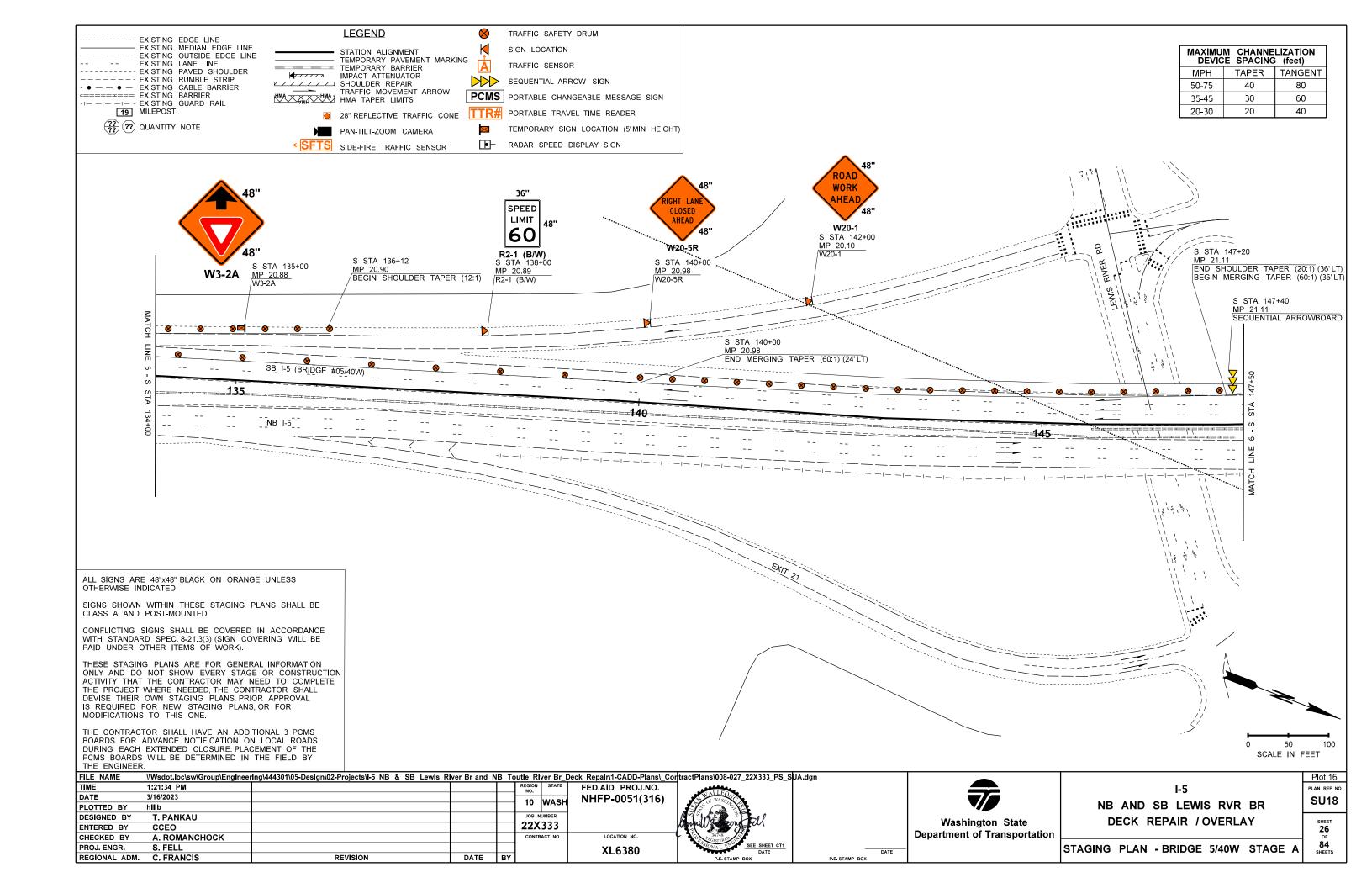


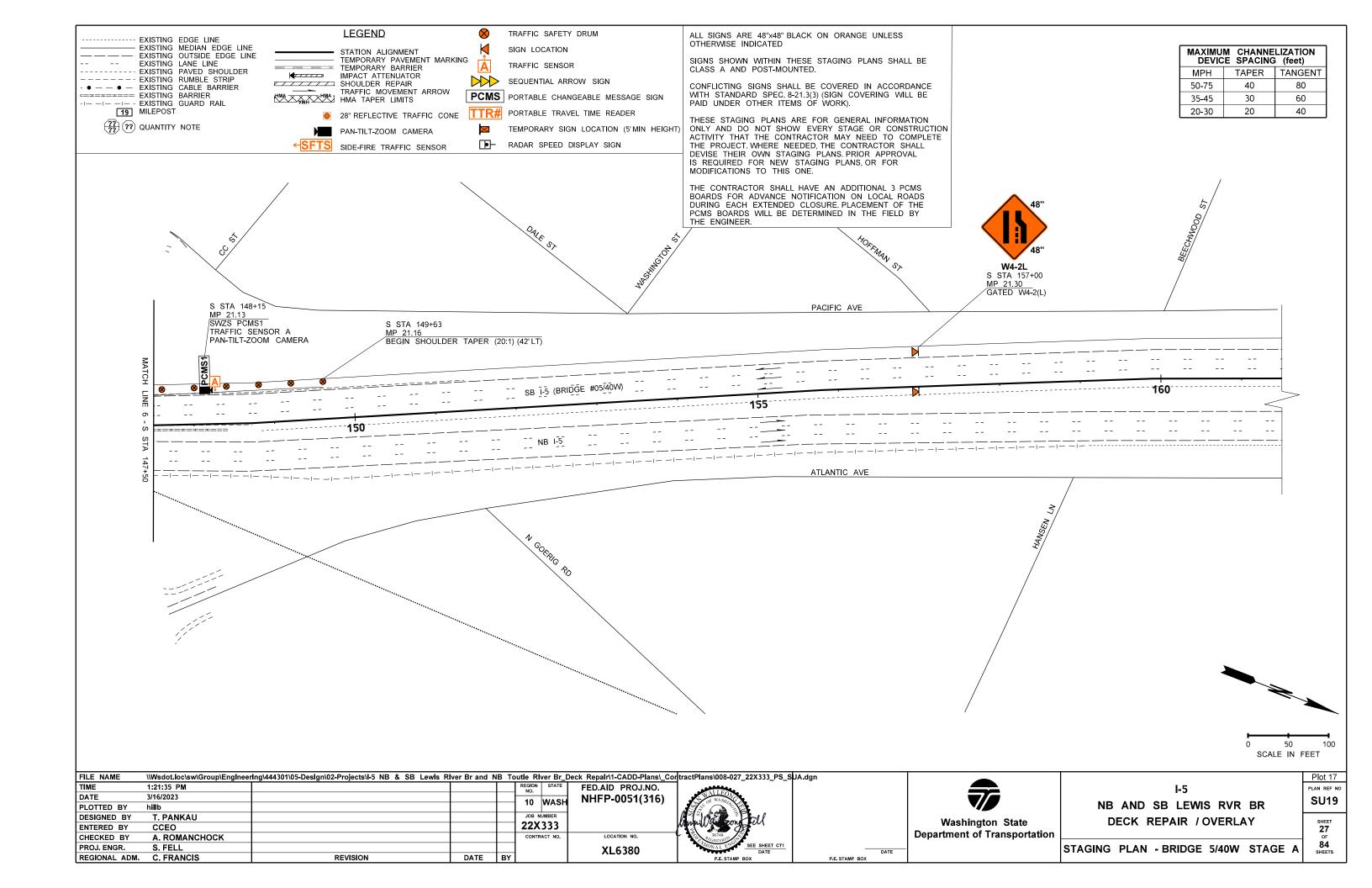


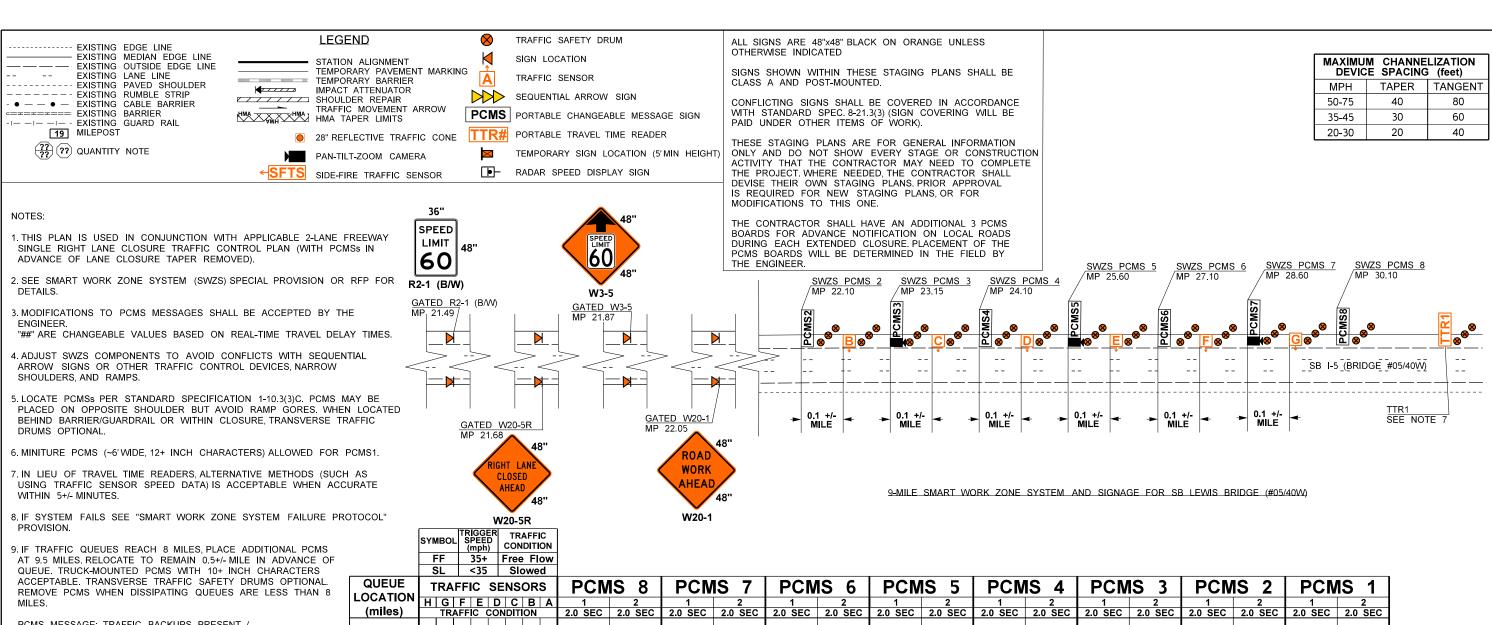










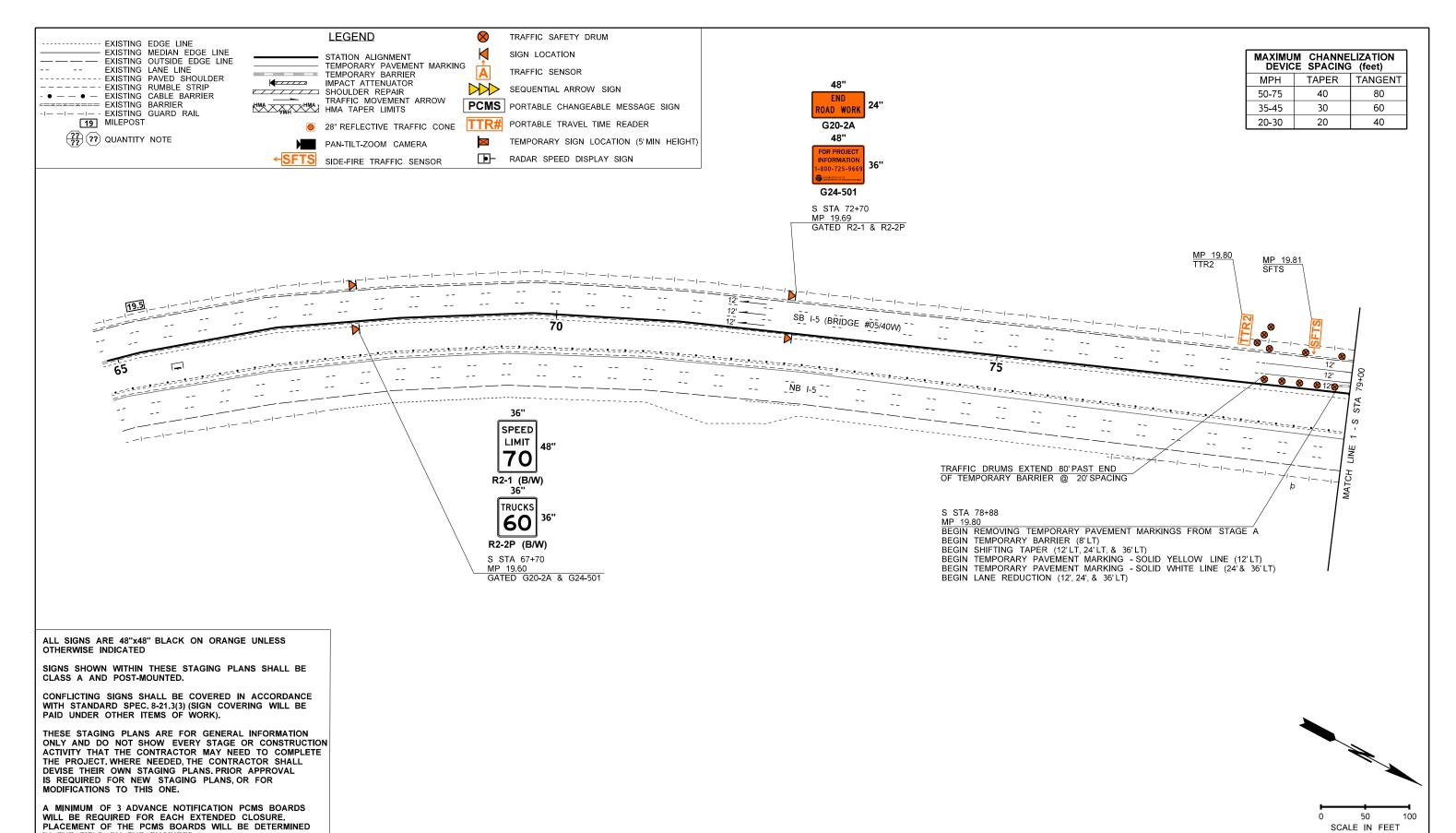


PCMS MESSAGE: TRAFFIC BACKUPS PRESENT / WATCH FOR SLOW TRAFFIC

10. AVOID PCMS PLACEMENT ON HORIZONTAL CURVES.

	•		Ţ (mph	i)	CO	NDITION																	
	F	FF		35+	-	Fre	e Flov	/																
	S	SL		< 35	5	S	lowed																	
QUEUE	T	ΓRA	FF	IC	SE	ENS	SORS	P	CN	IS 8	PCN	1S 7	PCN	1S 6	PCN	1S 5	PCN	1S 4	PCM	IS 3	PCN	1S 2	PCN	IS 1
LOCATION	Н	G	F	E	D	T	BA	1		2	1	2	1	2	1	2	1	2	1	2	1	2	1	2
(miles)		TR	AFF	IC	COI	NDI	TION	2.0 5	EC	2.0 SEC	2.0 SEC	2.0 SEC	2.0 SEC	2.0 SEC	2.0 SEC	2.0 SEC	2.0 SEC	2.0 SEC	2.0 SEC	2.0 SEC	2.0 SEC	2.0 SEC	2.0 SEC	2.0 SEC
None	FF	FF	FF	FF	FF	F	FF			(Blank)		(Blank)		(Blank)		(Blank)		(Blank)		(Blank)	RIGHT LANE CLOSED	1 MILE AHEAD		(Blank)
		\vdash			+	+		1.	_		l		1		1		SINGLE	3	TRAFFIC	##	SLOW OR	NEXT	l	
0.5 TO 1.4	FF	FF	FF	FF	FF	F	FSL	-	_	(Blank)		(Blank)		(Blank)		(Blank)	LANE	MILES	BACKUPS	MINUTE	STOPPED	1		(Blank)
		l	-	ļ	1	1.	.		•	` '		` '		` '		` ′	CLOSURE	AHEAD	PRESENT	DELAY	TRAFFIC	MILE		, ,
															SINGLE	4.5	TRAFFIC	##	SLOW OR	NEXT	ZIPPER	USE	ZIPPER	TAKE
1.41 TO 2.4	FF	FF	FF	FF	FF	: S	L SL			(Blank)		(Blank)		(Blank)	LANE	MILES	BACKUPS	MINUTE	STOPPED	2	MERGE	RIGHT	MERGE	TURNS
								•	•						CLOSURE	AHEAD	PRESENT	DELAY	TRAFFIC	MILES	AHEAD	LANE TOO	HERE	
		l				l	.						SINGLE	6	TRAFFIC	##	SLOW OR	NEXT	2 MILES	USE	ZIPPER	USE	ZIPPER	TAKE
2.41 TO 3.4	FF	FF	FF	FF	∣SL	. S	L SL	1_	_	(Blank)	l	(Blank)	LANE	MILES	BACKUPS	MINUTE	STOPPED	3	TO MERGE		MERGE	RIGHT	MERGE	TURNS
		├			-	+	+	╀-	-		0111015		CLOSURE	AHEAD	PRESENT	DELAY	TRAFFIC	MILES	POINT	3 LANES	AHEAD	LANE TOO	HERE	TA1/5
3.41 TO 4.9				6.	61			•		(Dlamb)	SINGLE LANE	7.5 MILES	TRAFFIC BACKUPS	## MINUTE	SLOW OR STOPPED	NEXT	3 MILES TO MERGE	USE	2 MILES	USE ALL	ZIPPER MERGE	USE RIGHT	ZIPPER MERGE	TAKE TURNS
3.41 10 4.9	FF		FF)SL	- SL	- S	ᄓᄓ	1.		(Blank)	CLOSURE	AHEAD	PRESENT	DELAY	TRAFFIC	4.5 MILES	POINT	ALL 3 LANES	TO MERGE POINT	3 LANES	AHEAD	LANE TOO	HERE	TURNS
		\vdash			1	+		SING	16	9	TRAFFIC	##	SLOW OR	NEXT	4.5 MILES	USE	3 MILES	USE	2 MILES	USE	ZIPPER	USE	ZIPPER	TAKE
4.91 TO 6.4	FE	FF	SI	SI	SI	S	ılsıl	LAN		MILES	BACKUPS	MINUTE	STOPPED	6	TO MERGE		TO MERGE		TO MERGE		MERGE	RIGHT	MERGE	TURNS
1.51 10 0.4	٠.	١		-	"	- "	-	CLOS		AHEAD	PRESENT	DELAY	TRAFFIC	MILES	POINT	3 LANES	POINT	3 LANES	POINT	3 LANES	AHEAD	LANE TOO	HERE	1011110
						Т		1 LA	NE	##	SLOW OR	NEXT	6 MILES	USE	4.5 MILES	USE	3 MILES	USE	2 MILES	USE	ZIPPER	USE	ZIPPER	TAKE
6.41 TO 7.9	FF	SL	SL	SL	SL	. S	LSL	CLOS	JRE	MINUTE	STOPPED	7.5	TO MERGE	ALL	TO MERGE	ALL	TO MERGE	ALL	TO MERGE	ALL	MERGE	RIGHT	MERGE	TURNS
								9 MII	.ES	DELAY	TRAFFIC	MILES	POINT	3 LANES	POINT	3 LANES	POINT	3 LANES	POINT	3 LANES	AHEAD	LANE TOO	HERE	
								SLOW		NEXT	1 LANE	##	6 MILES	USE	4.5 MILES	USE	3 MILES	USE	2 MILES	USE	ZIPPER	USE	ZIPPER	TAKE
7.91+	SL	SL	SL	SL	.∣SL	. S	L SL	STOP		9	CLOSED	MINUTE	TO MERGE		TO MERGE		TO MERGE		TO MERGE		MERGE	RIGHT	MERGE	TURNS
								TRAF	FIC	MILES	7.5 MILES	DELAY	POINT	3 LANES	POINT	3 LANES	POINT	3 LANES	POINT	3 LANES	AHEAD	LANE TOO	HERE	

FILE NAME	\\Wsdot.loc\sw\Group\Englnee	ring\444301\05-Design\02-Projects\I-5 NB & SB Lewis R	Iver Br and	NB To	outle River Br_D	Deck Repair\1-CADD-Plans_Co	rtractPlans\008-027_22X333_PS_S	SUA.dgn			Plot 18
TIME	1:21:36 PM				REGION STATE	FED.AID PROJ.NO.	ميبيون			I-5	PLAN REF NO
DATE	3/16/2023				10 WASH	NHFP-0051(316)	WASH				SU20
PLOTTED BY	hillb				IU WASH					NB AND SB LEWIS RVR BR	5020
DESIGNED BY	T. PANKAU				JOB NUMBER				Washington State	DECK REPAIR / OVERLAY	SHEET
ENTERED BY	CCEO				22X333	,	Minimum		9		28
CHECKED BY	A. ROMANCHOCK				CONTRACT NO.	LOCATION NO.	36748		Department of Transportation		_ OF
PROJ. ENGR.	S. FELL					XL6380	SEE SHEET CT1	DATE	-	STAGING PLAN - BRIDGE 5/40W STAGE A	84
REGIONAL ADM	. C. FRANCIS	REVISION	DATE	BY		AL0300	P.E. STAMP BOX	P.E. STAMP BOX			3112213



IN THE FIELD BY THE ENGINEER. FILE NAME

S. FELL

REGIONAL ADM. C. FRANCIS

PROJ. ENGR.

\\Wsdot.loc\sw\Group\EnglneerIng\444301\05-Design\02-Projects\1-5 NB & SB Lewis River Br and NB Toutle River Br_Deck Repair\1-CADD-Plans_CortractPlans\014-035_22X333_PS_S|JB.dgn TIME FED.AID PROJ.NO. DATE 3/16/2023 NHFP-0051(316) PLOTTED BY hillb JOB NUMBER DESIGNED BY T. PANKAU 22X333 ENTERED BY CCEO CHECKED BY A. ROMANCHOCK LOCATION NO.

DATE

BY

REVISION



XL6380

Washington State **Department of Transportation**

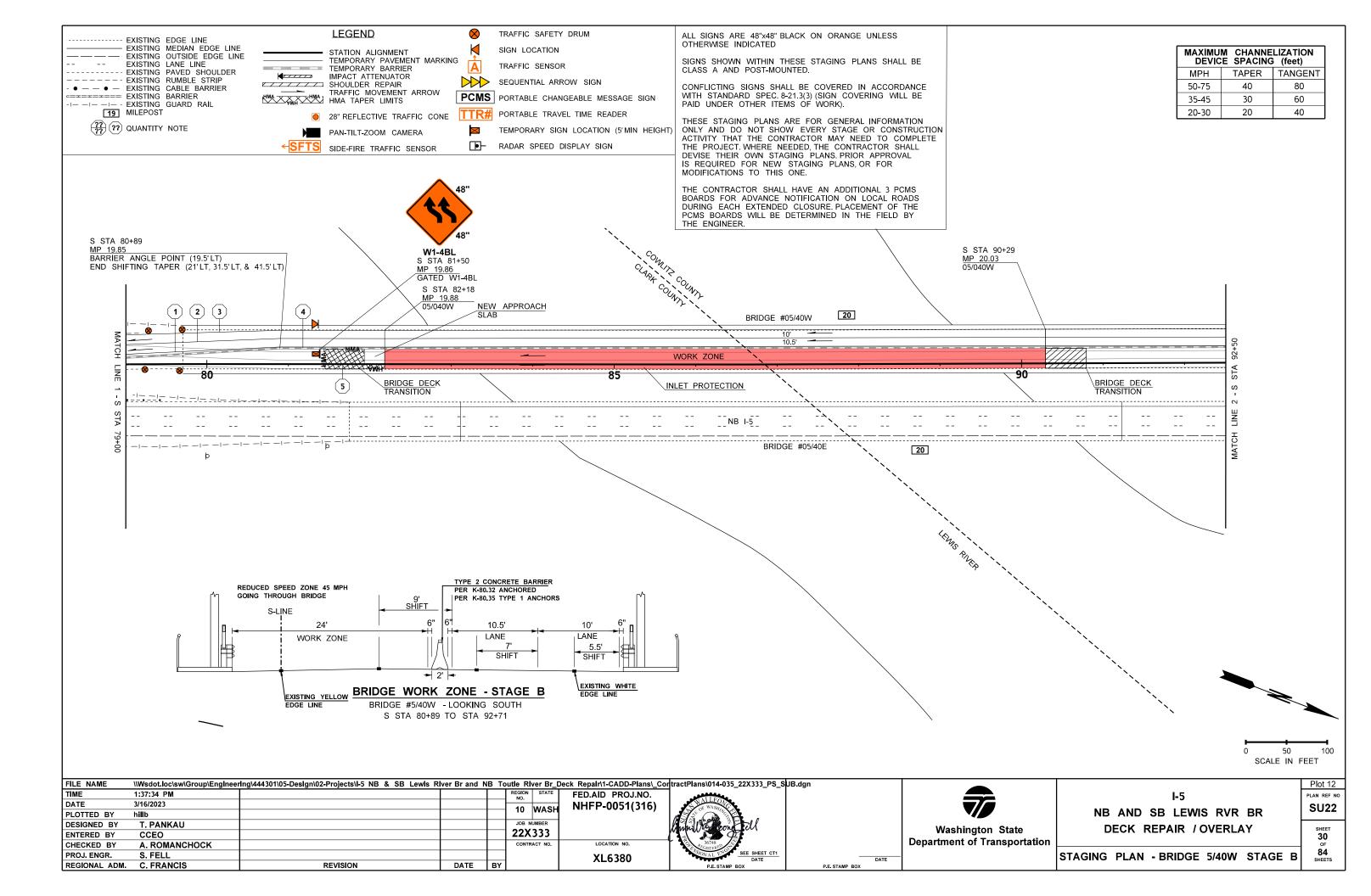
DATE

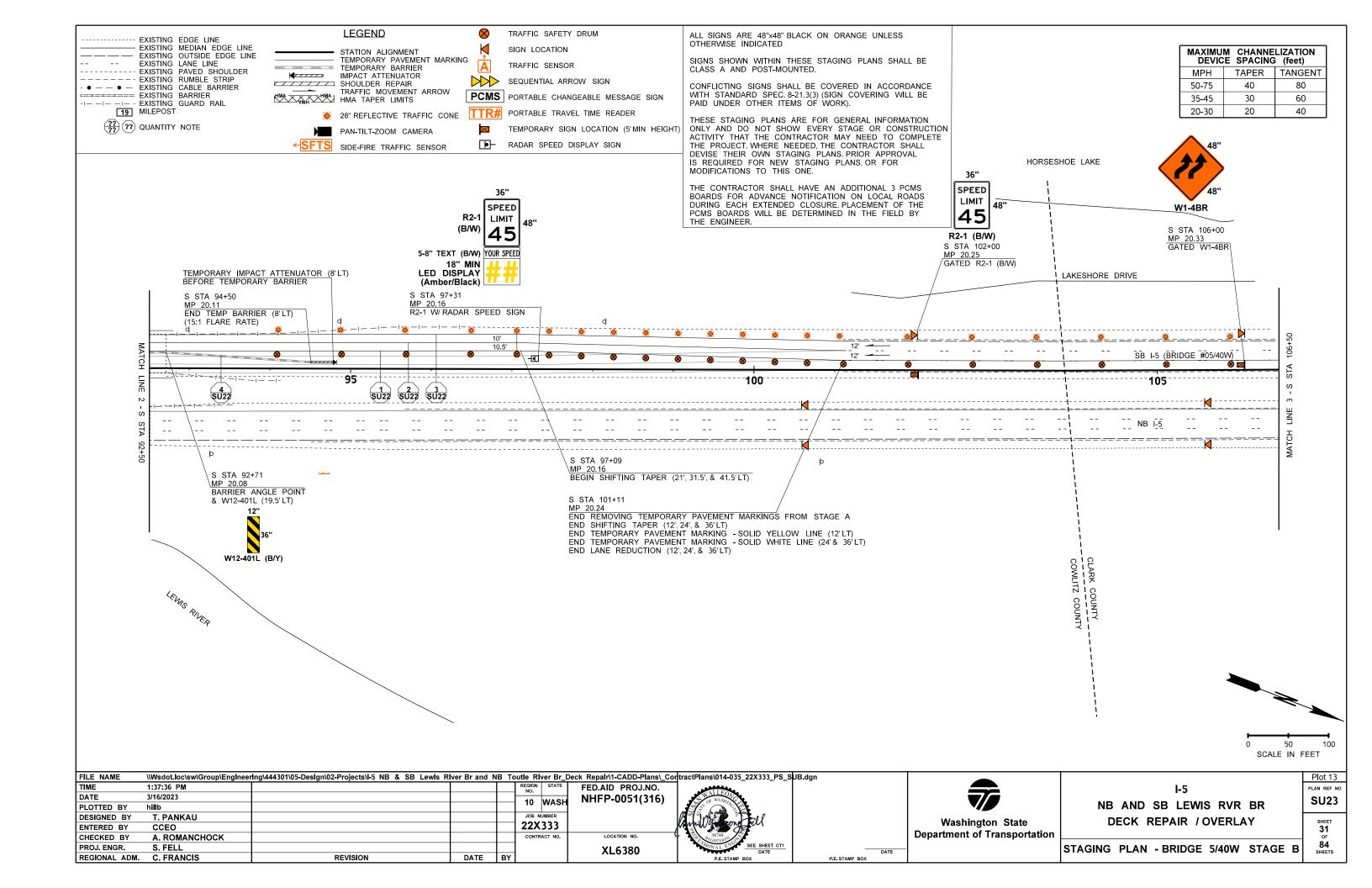
I-5 NB AND SB LEWIS RVR BR **DECK REPAIR / OVERLAY**

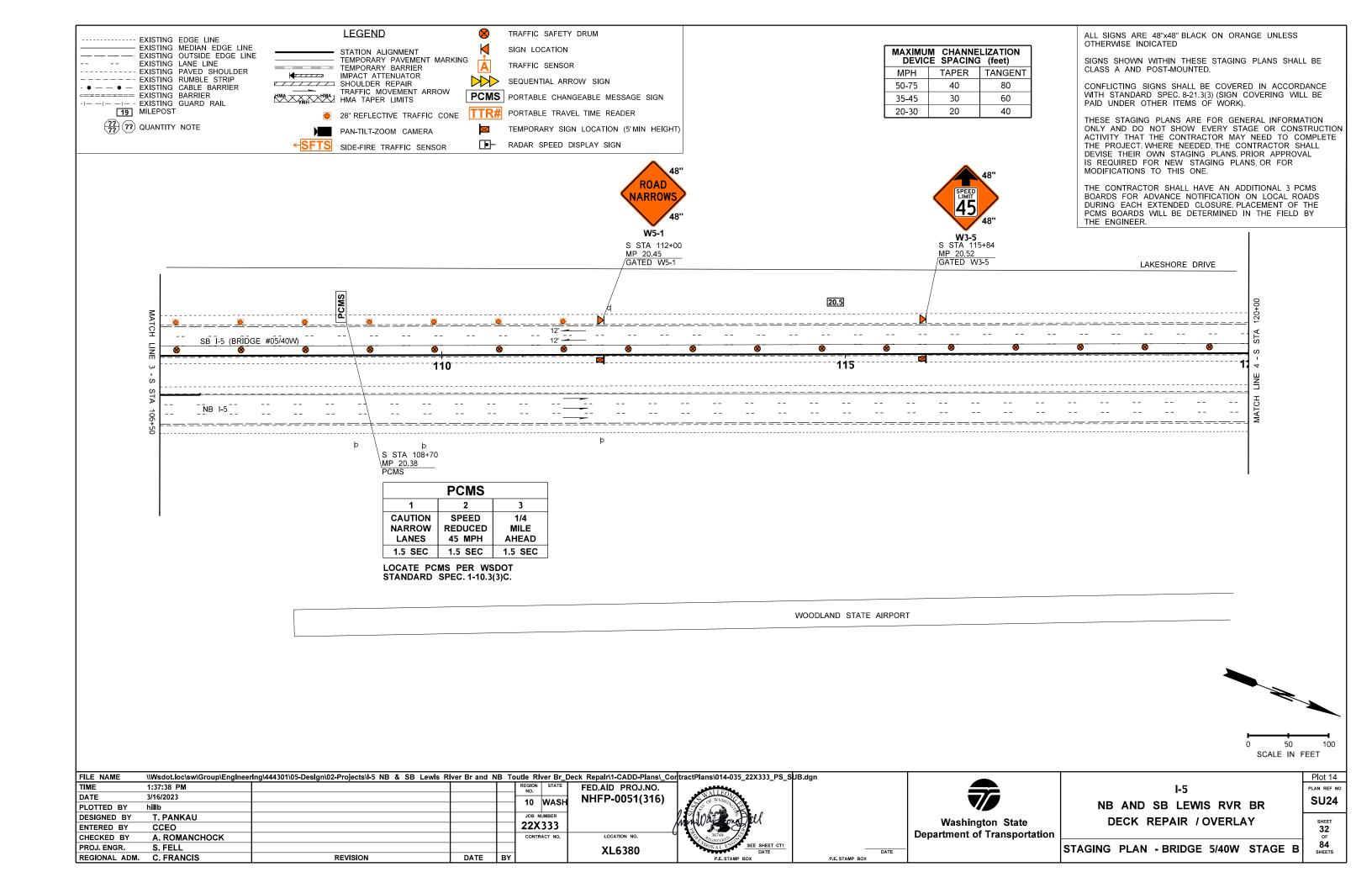
STAGING PLAN - BRIDGE 5/40W STAGE B

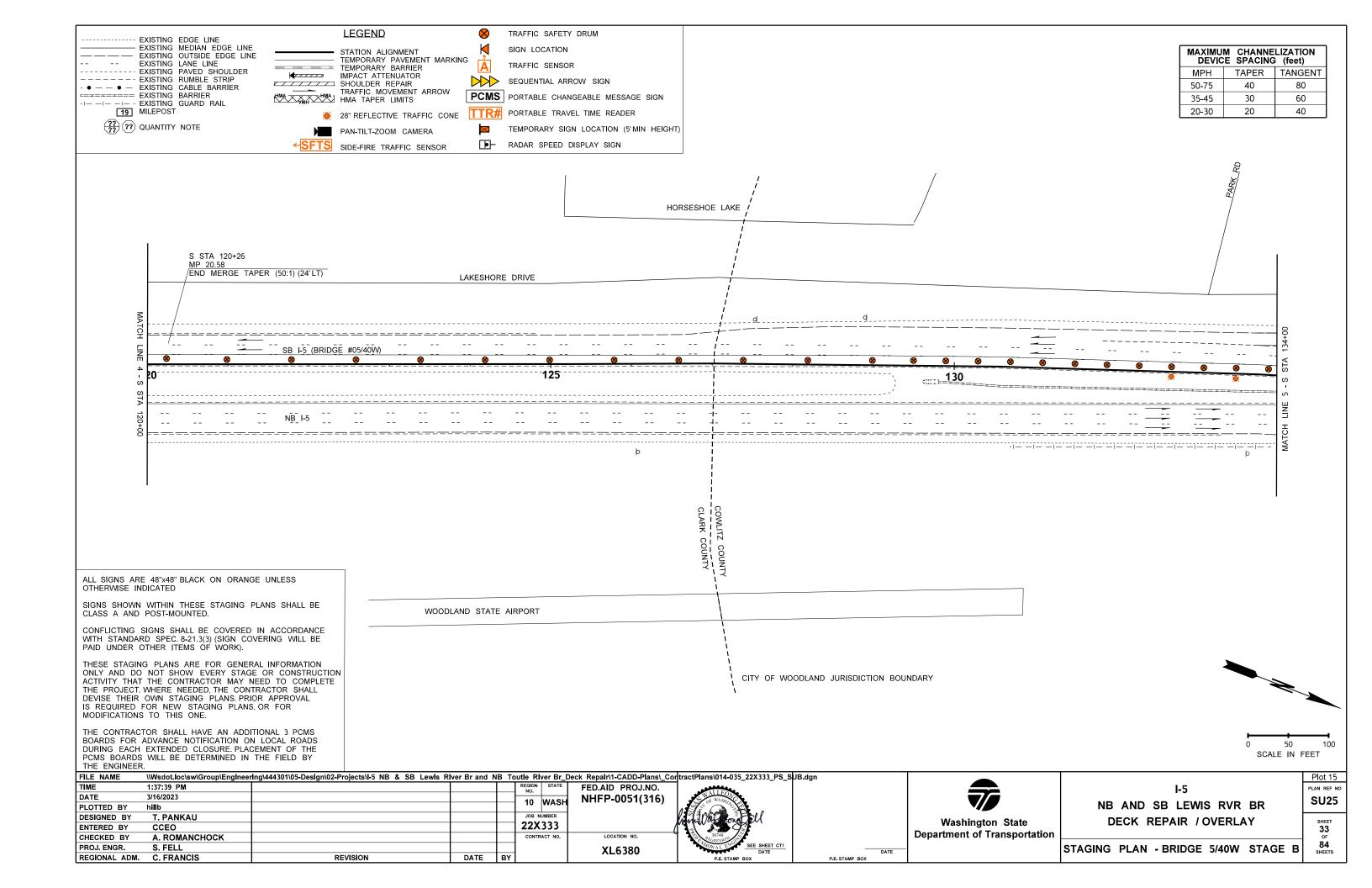
Plot 11 PLAN REF N **SU21** 3HEET **29**

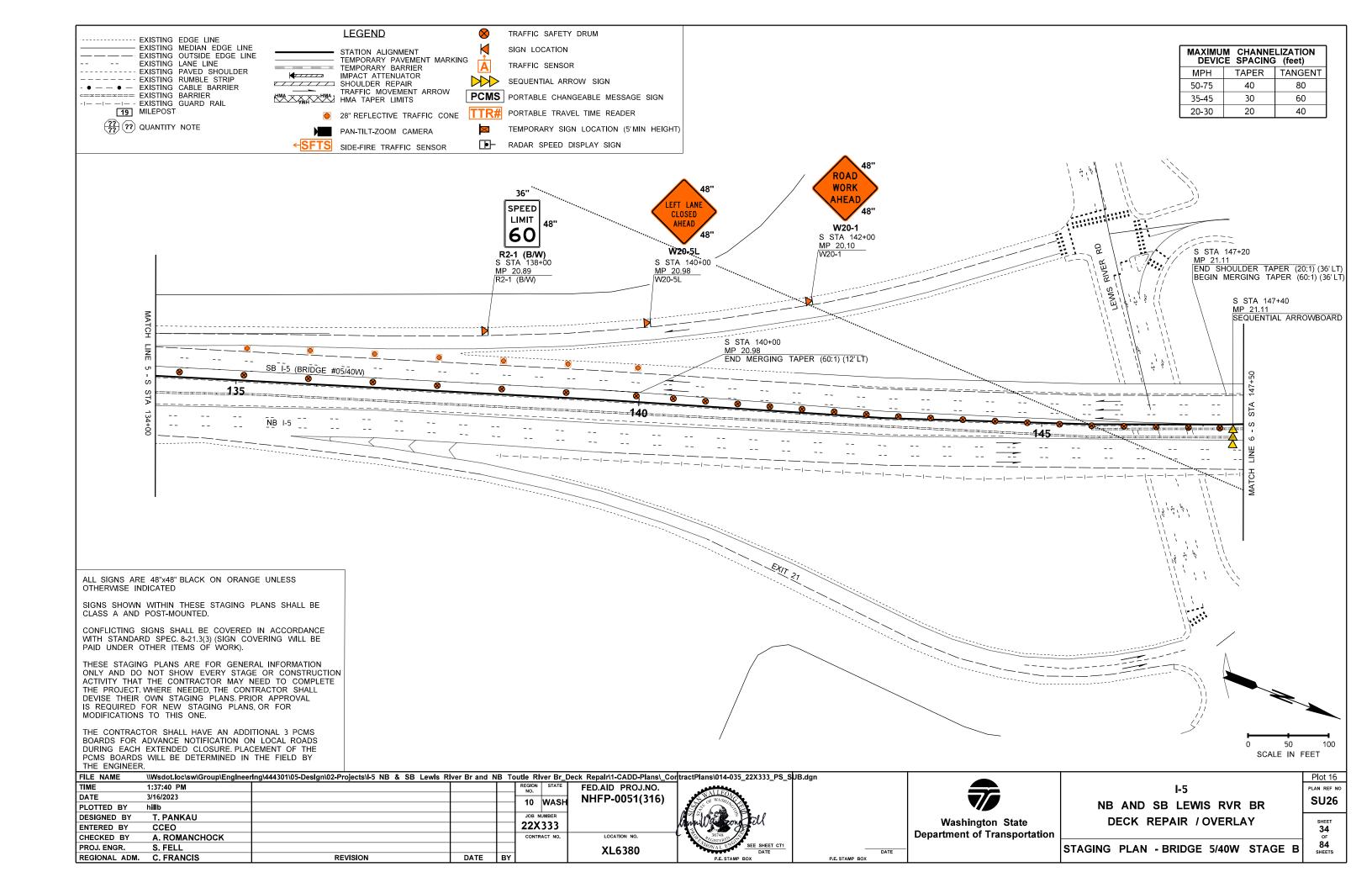
84 SHEETS

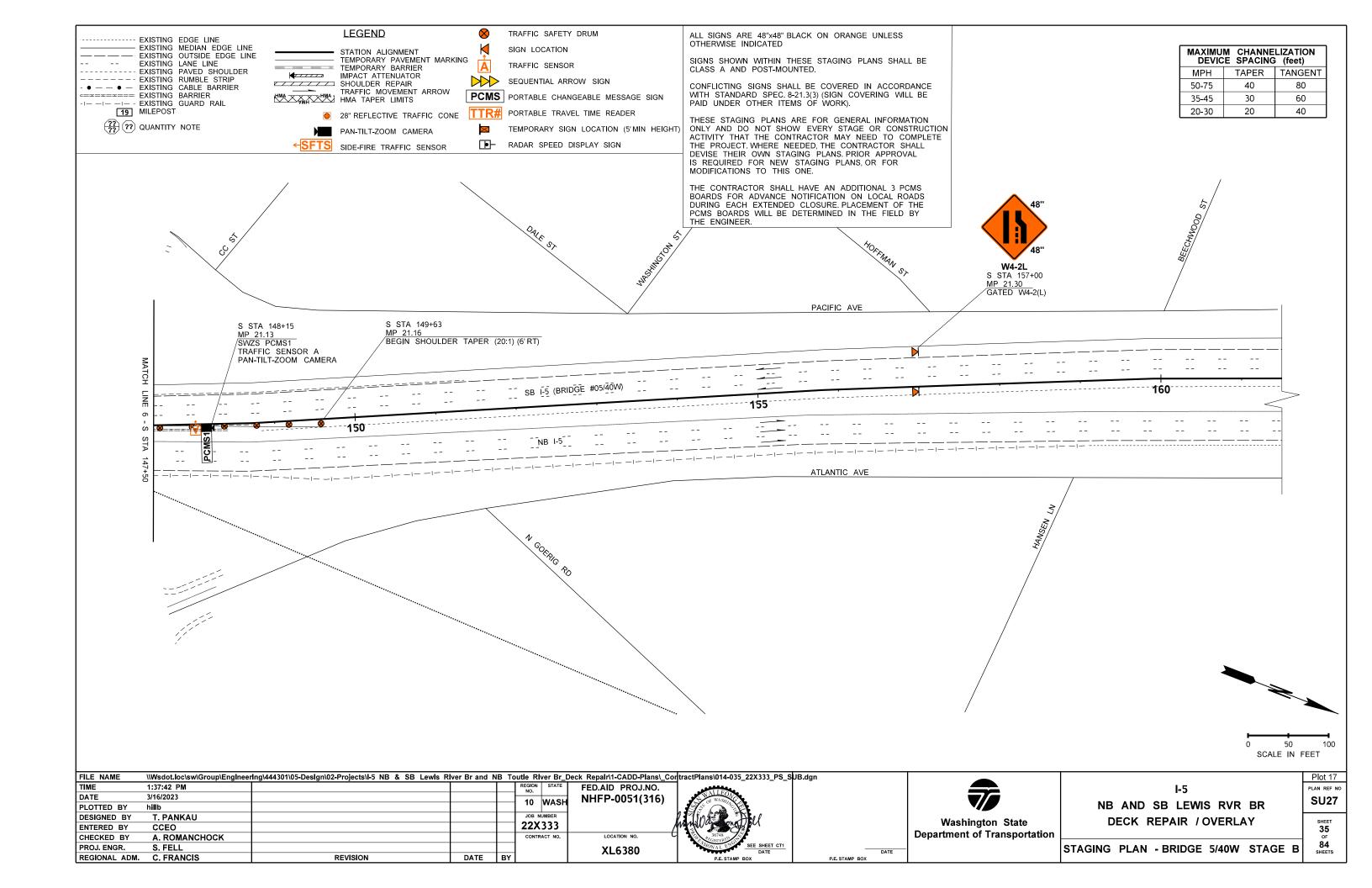


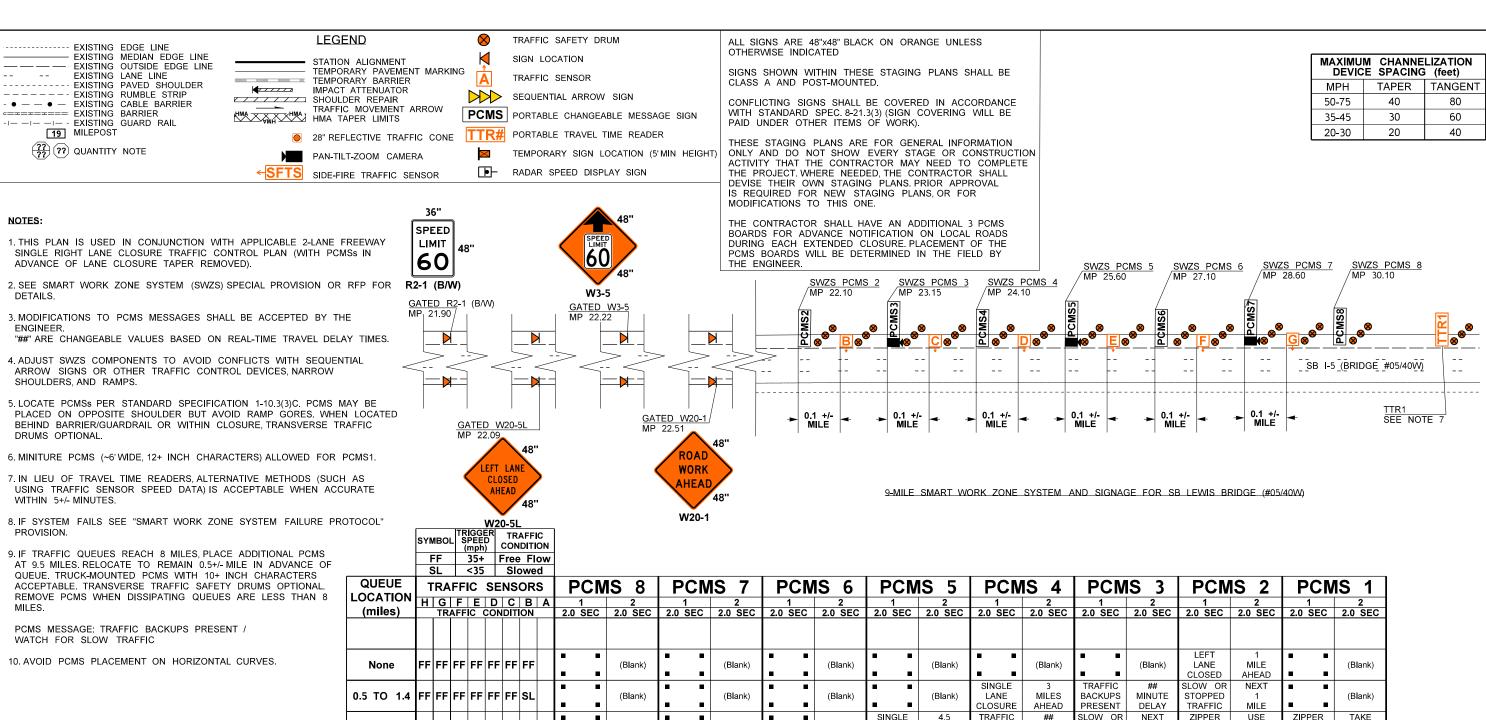






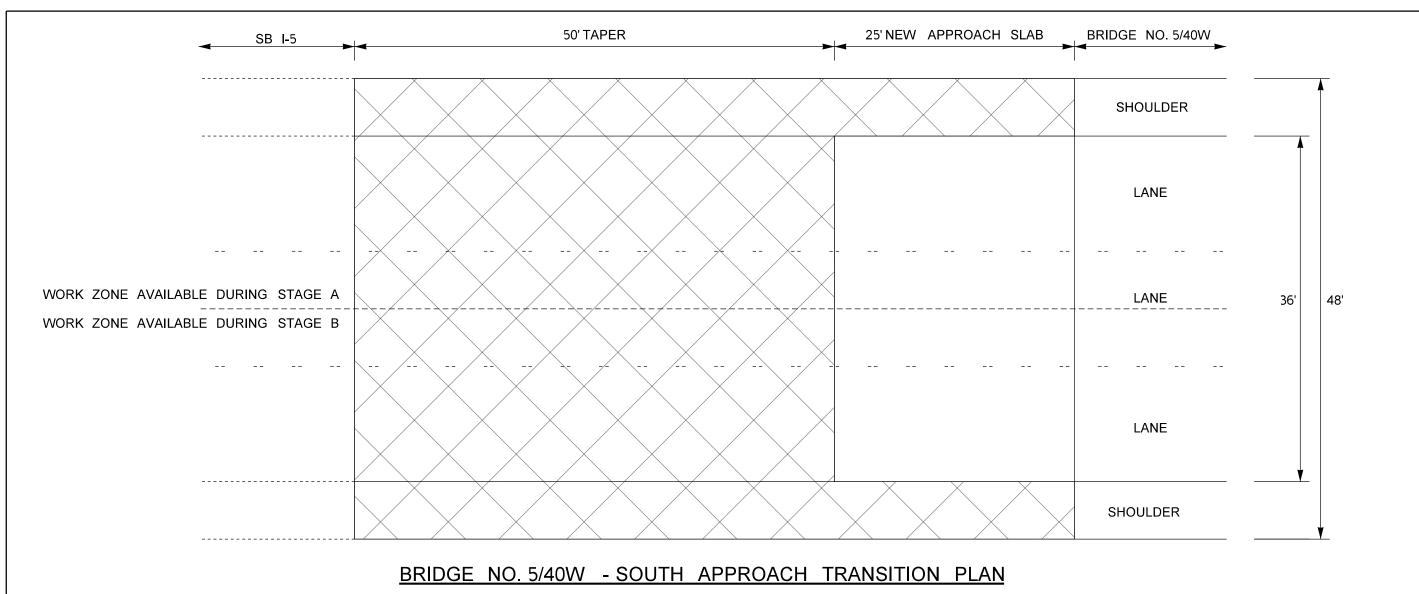


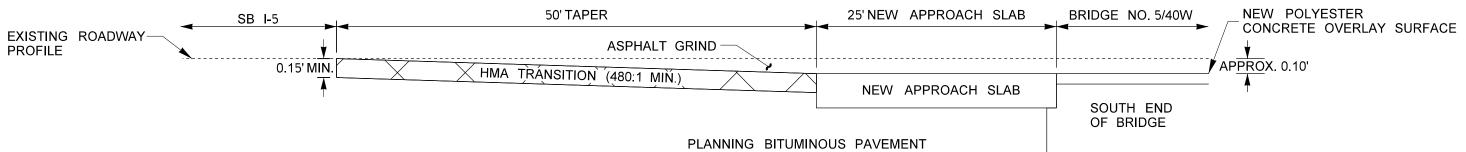




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QUEUE		TR	AFF	IC	S	ENS	SOR	s	PCM	IS 8	PCN	IS 7	PCN	IS 6	PCN	IS 5	PCN	IS 4	PCM	IS 3	PCN	1S 2	PCN	1 S 1
LOCATION	۲П	ΠG	F	ΙE		О	; B	Α	1	2	1	2	1	2	1	2	1	2	1	2	1	2	1	2
(miles)			RAFI	FIC.	CO	NDI	TION		2.0 SEC	2.0 SEC	2.0 SEC	2.0 SEC	2.0 SEC	2.0 SEC	2.0 SEC	2.0 SEC	2.0 SEC	2.0 SEC	2.0 SEC	2.0 SEC	2.0 SEC	2.0 SEC	2.0 SEC	2.0 SEC
None	F	FI	FFF	FF	F	F	FFF		• •	(Blank)		(Blank)		(Blank)		(Blank)	-	(Blank)		(Blank)	LEFT LANE	1 MILE		(Blank)
																					CLOSED	AHEAD		
																	SINGLE	3	TRAFFIC	##	SLOW OR	NEXT		
0.5 TO 1.4	4 FI	= FI	F FF	FF	F FI	F FF	F SL			(Blank)		(Blank)		(Blank)		(Blank)	LANE	MILES	BACKUPS	MINUTE	STOPPED	1		(Blank)
		\perp															CLOSURE	AHEAD	PRESENT	DELAY	TRAFFIC	MILE	-	
		_		.		_									SINGLE	4.5	TRAFFIC	##	SLOW OR	NEXT	ZIPPER	USE	ZIPPER	TAKE
1.41 TO 2.	4 FI	= FI	F FF	FF	FIFI	F∣SI	L SL			(Blank)		(Blank)		(Blank)	LANE	MILES	BACKUPS	MINUTE	STOPPED	2	MERGE	LEFT	MERGE	TURNS
	_	\bot		_											CLOSURE	AHEAD	PRESENT	DELAY	TRAFFIC	MILES	AHEAD	LANE TOO		
	.ا	_	_	.		.	.						SINGLE	6	TRAFFIC	##	SLOW OR	NEXT	2 MILES	USE	ZIPPER	USE	ZIPPER	TAKE
2.41 TO 3.	4 +1	- +1	-	F F	- SI	L SI	L SL			(Blank)		(Blank)	LANE	MILES	BACKUPS	MINUTE	STOPPED	3	TO MERGE		MERGE	LEFT	MERGE	TURNS
	_	+	_	+	+	_		\vdash					CLOSURE	AHEAD	PRESENT	DELAY	TRAFFIC	MILES	POINT	3 LANES	AHEAD	LANE TOO	HERE	T.1./F
3.41 TO 4.	ᆚᆫ	-	-			.				(Dlamb)	SINGLE LANE	7.5 MILES	TRAFFIC BACKUPS	## MINUTE	SLOW OR STOPPED	NEXT	3 MILES TO MERGE	USE ALL	2 MILES TO MERGE	USE ALL	ZIPPER MERGE	USE LEFT	ZIPPER MERGE	TAKE TURNS
3.41 10 4.	اعالد	יין־ן	-	. Sr	- SI	L SI	L SL			(Blank)	CLOSURE	AHEAD	PRESENT	DELAY	TRAFFIC	4.5 MILES	POINT	3 LANES	POINT	3 LANES	AHEAD	LANE TOO	HERE	TURNS
	+	+	-	+	+	+			SINGLE	9	TRAFFIC	##	SLOW OR	NEXT	4.5 MILES	USE	3 MILES	USE	2 MILES	USE	ZIPPER	USE	ZIPPER	TAKE
4.91 TO 6.	ᆀᇎ	- - 1	E SI	SI.	9	ı sı	LSL		LANE	MILES	BACKUPS	MINUTE	STOPPED	6	TO MERGE	ALL	TO MERGE	ALL	TO MERGE	ALL	MERGE	LEFT	MERGE	TURNS
4.51 10 0.	''''	١	31	56	- "	- 3	- 3-		CLOSURE	AHEAD	PRESENT	DELAY	TRAFFIC	MILES	POINT	3 LANES	POINT	3 LANES	POINT	3 LANES	AHEAD	LANE TOO	HERE	1011110
	1	+	+			+			1 LANE	##	SLOW OR	NEXT	6 MILES	USE	4.5 MILES	USE	3 MILES	USE	2 MILES	USE	ZIPPER	USE	ZIPPER	TAKE
6.41 TO 7	9 FI	= si	LISL	SL	S	∟İsı	LSL		CLOSURE	MINUTE	STOPPED	7.5	TO MERGE	ALL	TO MERGE	ALL	TO MERGE	ALL	TO MERGE	ALL	MERGE	LEFT	MERGE	TURNS
	Ţ.,	٦.	- -	-	٦,	-ات.	_		9 MILES	DELAY	TRAFFIC	MILES	POINT	3 LANES	POINT	3 LANES	POINT	3 LANES	POINT	3 LANES	AHEAD	LANE TOO	HERE	
	1	\top							SLOW OR	NEXT	1 LANE	##	6 MILES	USE	4.5 MILES	USE	3 MILES	USE	2 MILES	USE	ZIPPER	USE	ZIPPER	TAKE
7.91+	İsı	₋lsı	LISL	. SL	S	LİSI	LSL		STOPPED	9	CLOSED	MINUTE	TO MERGE	ALL	TO MERGE	ALL	TO MERGE	ALL	TO MERGE	ALL	MERGE	LEFT	MERGE	TURNS
	1								TRAFFIC	MILES	7.5 MILES	DELAY	POINT	3 LANES	POINT	3 LANES	POINT	3 LANES	POINT	3 LANES	AHEAD	LANE TOO	HERE	

FILE NAME	\\Wsdot.loc\sw\Group\Englnee	ring\444301\05-Design\02-Projects\I-5 NB & SB Lewis R	lver Brand N	IB Toutle River Br_D	eck Repair\1-CADD-Plans_Co	tractPlans\014-035_22X333_PS_S	UB.dgn			Plot 18
TIME	1:37:44 PM			REGION STATE	FED.AID PROJ.NO.	appeara.			I-5	PLAN REF NO
DATE	3/16/2023			10 WASH	NHFP-0051(316)	OF WASH C				SU28
PLOTTED BY	hillb			I IU WASH					NB AND SB LEWIS RVR BR	0020
DESIGNED BY	T. PANKAU			JOB NUMBER		VIII)		Washington State	DECK REPAIR / OVERLAY	SHEET
ENTERED BY	CCEO			22X333	/	Wall was a second		3	DECK KEFAIK / OVERLAT	36
CHECKED BY	A. ROMANCHOCK			CONTRACT NO.	LOCATION NO.	36748 PEGISTERYS		Department of Transportation		OF
PROJ. ENGR.	S. FELL				XL6380	SEE SHEET CT1	DATE		STAGING PLAN - BRIDGE 5/40W STAGE B	84
REGIONAL ADM	C FRANCIS	REVISION	DATE	RY	ALUJUU	DE STAMP POY	DE STAMP POY			SHEETS



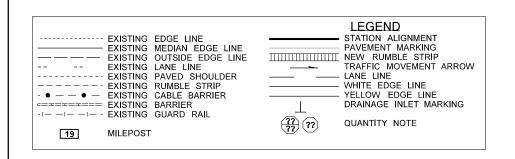


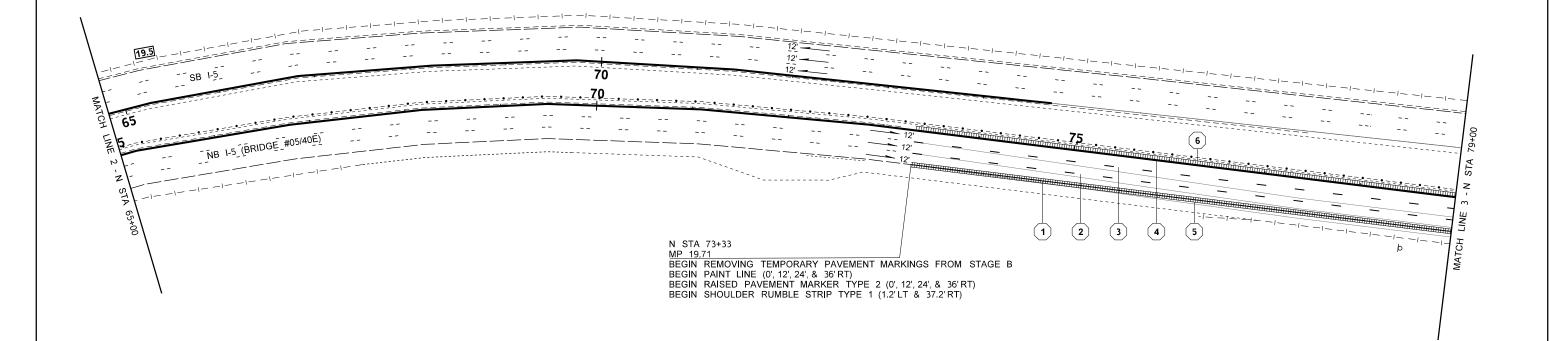
BRIDGE NO. 5/40W - SOUTH APPROACH TRANSITION PROFILE

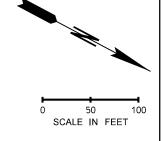
- 1. PROFILE FEATURES EXAGGERATED FOR CLARIFY, NOT TO SCALE 2. MATCH SHOULDER GRADE TO TRAVELED WAY

FILE NAME	\\Wsdot.loc\sw\Group\Enginee	ring\444301\05-Design\02-Projects\i-5 NB & SB Lewis R	Iver Brand I	NB Toutle River Br_D	Deck Repair\1-CADD-Plans_Co	ontractPlans\036_22X333_PS_RD.dg	j n			Plot 1
TIME	3:45:15 PM			REGION STATE	FED.AID PROJ.NO.	NUE			I-5	PLAN REF NO
DATE	3/22/2023			10 WASH	NHFP-0051(316)	OF WASHING				RD1
PLOTTED BY	hillb			I IU WASH	,				NB AND SB LEWIS RVR BR	
DESIGNED BY	T. PANKAU			JOB NUMBER		minustra seones el		Washington State	DECK REPAIR / OVERLAY	SHEET
ENTERED BY	CCEO			22X333						37
CHECKED BY	A. ROMANCHOCK			CONTRACT NO.	LOCATION NO.	36748 REGISTER SO		Department of Transportation		OF
PROJ. ENGR.	S. FELL				XL6380	SEE SHEET CT1	DATE		ROADWAY PROFILE- BRIDGE TRANSITION	84
REGIONAL ADM.	C. FRANCIS	REVISION	DATE	BY	X20300	P.E. STAMP BOX	P.E. STAMP BOX		NOADWAT TROTIEE BRIDGE TRANSMICK	SHEETS

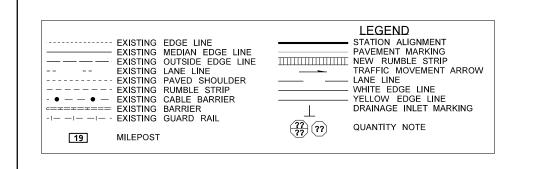
				QUA	NTIT	Y TA	BUL	ATIO	N - F	PAVE	MEN	T MA	RKII	١G				
NOTE: THE FIRST NUMBER OF THE "CODE" BELOW REFERS TO THE SHEET NO. OR THE SHEET REFERENCE NO. SHOWING THE CONSTRUCTION FEATURE. THE SECOND NUMBER REFERS TO THE CONSTRUCTION FEATURE FOUND ON THAT SHEET.	PAINT LINE	PLASTIC DRAINAGE MARKING	SHOULDER RUMBLE STRIP TYPE 1	E RAISED PAVEMENT MARKER												SEE GENERAL NOTES	GENERAL NOTES:	
CODE LOCATION Y \ UNIT OF MEASURE > MK1-1 N 73+33 TO 94+51 (36' RT) MK1-2 N 73+33 TO 95+43 (24' RT) MK1-3 N 73+33 TO 95+43 (12' RT) MK1-4 N 73+33 TO 94+51 (0') MK1-5 N 73+33 TO 80+74 (37.2' LT) MK1-6 N 73+33 TO 80+74 (37.2' LT) MK2-1 DRAINAGE INLETS MK3-1 N 92+51 TO 95+67 (1.2' LT) MK3-2 N 93+11 TO 94+51 (37.2' RT) MK4-1 S 78+88 TO 101+11 (0') MK4-2 S 78+04 TO 101+11 (12' LT) MK4-3 S 78+04 TO 101+11 (24' LT) MK4-4 S 78+88 TO 101+11 (36' LT) MK4-5 S 77+41 TO 79+70 (1.2' RT) MK5-1 DRAINAGE INLETS MK6-1 S 93+70 TO 101+11 (37.2' LT) MK6-2 S 92+71 TO 101+11 (1.2' RT)	2230 2310 2210 2120 2120	58 58	0.2 0.2 0.1 0.1	0.6 0.6 0.6 0.6 0.6 0.6 0.6 0.6 0.6												1,2,5 1,2,5 1,2,5 1,2,5 3 3 4 3 1,2,5 1,2,5 1,2,5 1,2,5 1,2,5 1,2,5 3 4 3 3 3	1. PAINT LINES PER STANDARD PLAN M-2 2. RAISED PAVEMENT MARKERS PER STAPLAN M-20.30. 3. SHOULDER RUMBLE STRIP TYPE 1 PER STANDARD PLAN M-60.10. 4. DRAINAGE STRUCTURE INLET MARKING STANDARD PLAN M-24.60. 5. ADDITIONAL RAISED PAVEMENT MARKING TYPE 2 QUANTITIES LOCATED ON QTSU STANDARD PLAN M-24.60.	ANDARD R G PER ER
SHEET TOTAL PROJECT TOTAL DESIGNED BY T. PANKAU ENTERED BY CCEO CHECKED BY A. ROMANCHOCK PROJ. ENGR. S. FELL REGION ADM. C. FRANCIS DATE DATE	. 17740	116 116	1.1	4.8 4.8	REGION NO. 10 JOB NU 22X CONTRA	333		PROJ. NO.				₹	Washington Departmen	s State t of Transp	ortation	QUANT	I-5 NB AND SB LEWIS RVR BR DECK REPAIR / OVERLAY TITY TABULATION - PAVEMENT MARKING	QTMK SHEE: 38 OF 84 SHEET

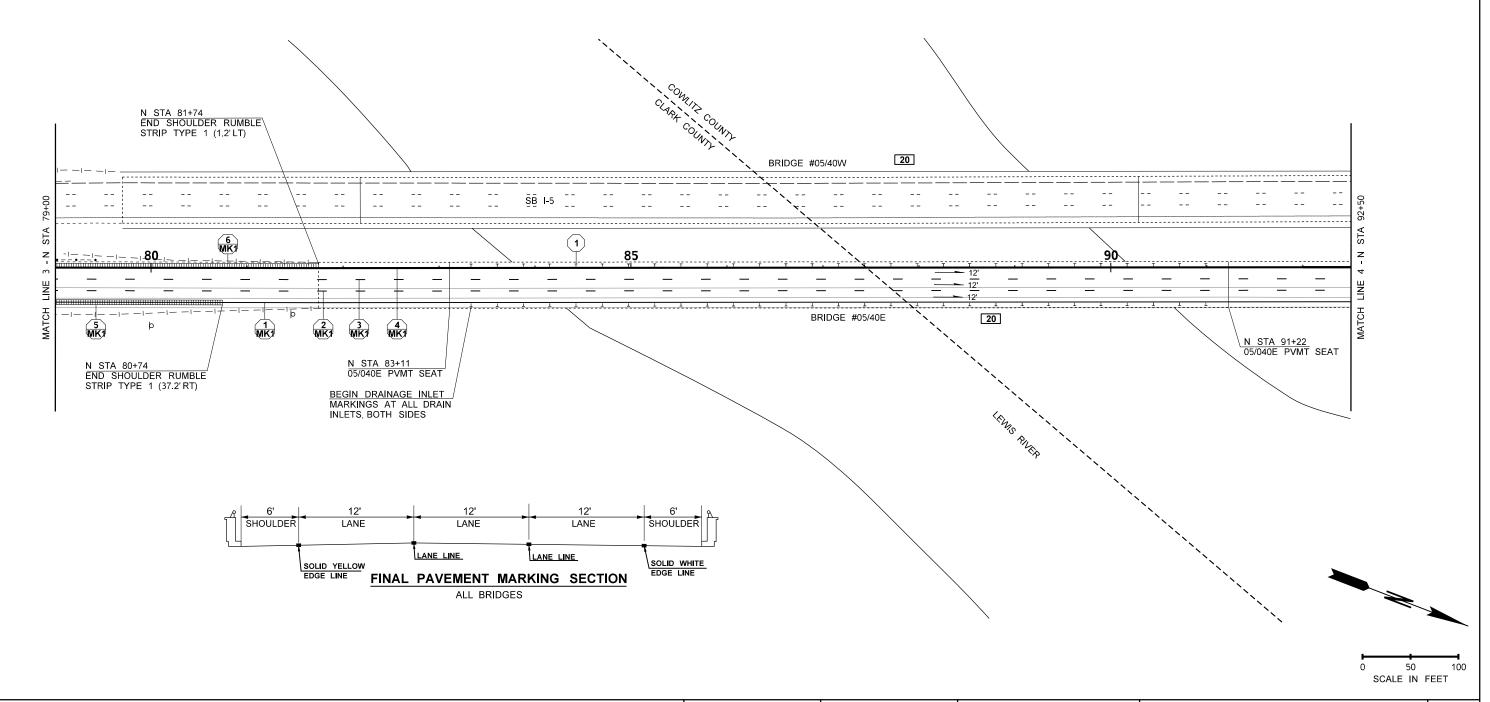




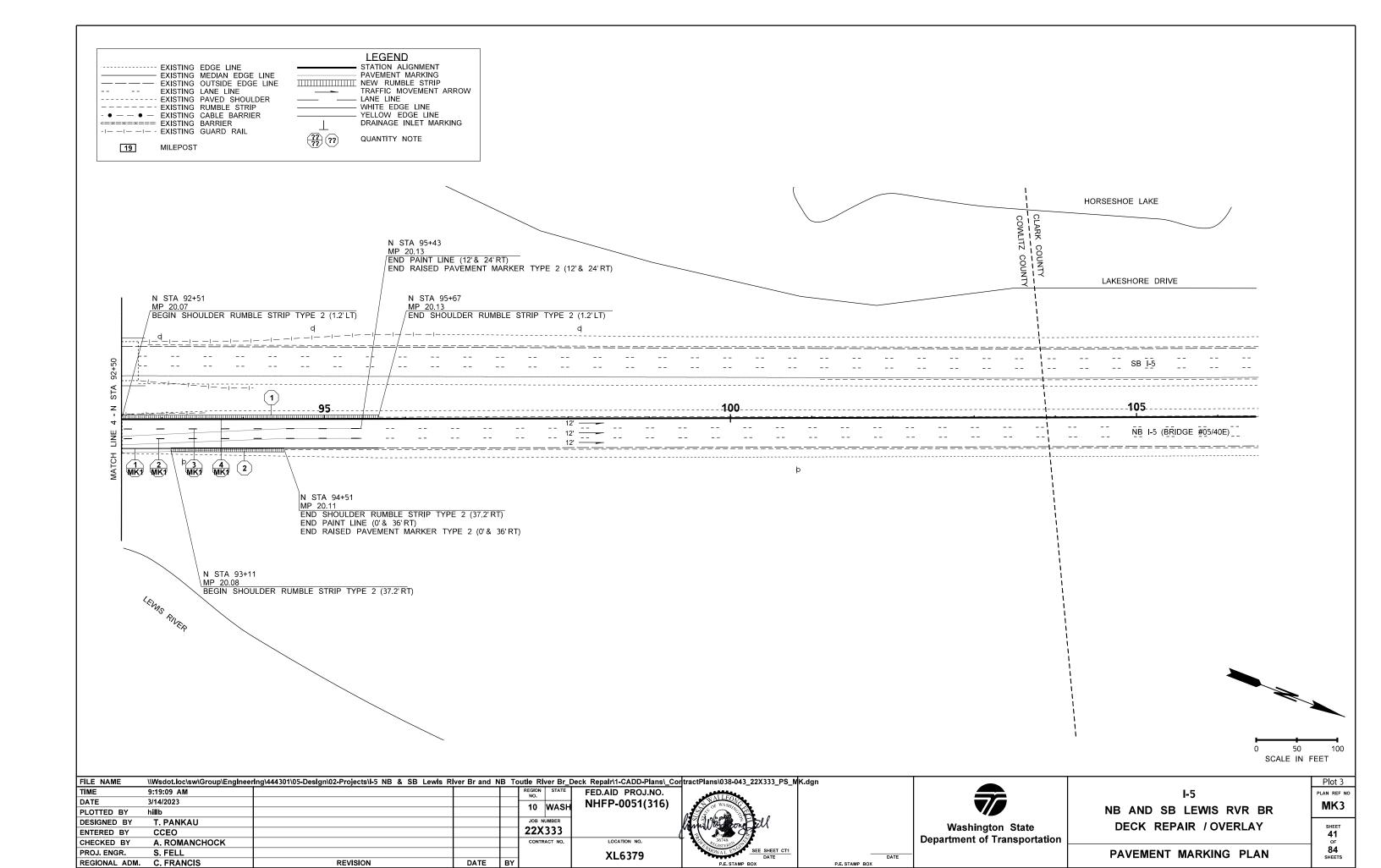


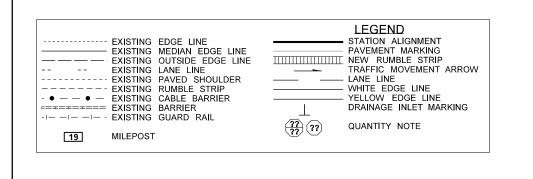
FILE NAME	\\Wsdot.loc\sw\Group\EngIneering\444301\05-DesIgn\02-Projects\I-5 NB &	SB Lewis River Br and NB To	utle River Br_C	Deck Repair\1-CADD-Plans_Co	rtractPlans\038-043_22X333_PS_MK.d	lgn			Plot 1
TIME	9:19:12 AM		REGION STATE	FED.AID PROJ.NO.	معبدو			I-5	PLAN REF NO
DATE	3/14/2023		10 WASH	NHFP-0051(316)	NATEON OF WASHING				MK1
PLOTTED BY	hillb		IU WASH	,				NB AND SB LEWIS RVR BR	"""\
DESIGNED BY	T. PANKAU		JOB NUMBER		And Day Brown		Washington State	DECK REPAIR / OVERLAY	SHEET
ENTERED BY	CCEO		22X333		William			DECK KEFAIK / OVERLAT	39
CHECKED BY	A. ROMANCHOCK		CONTRACT NO.	LOCATION NO.	36748 PEGISTERED		Department of Transportation		OF
PROJ. ENGR.	S. FELL			XL6379	SIONAL EN SEE SHEET CT1	DATE		PAVEMENT MARKING PLAN	84 SHEETS
REGIONAL ADM.	C. FRANCIS REVISION	DATE BY		ALOSIS	P.E. STAMP BOX	P.E. STAMP BOX			SHEETS





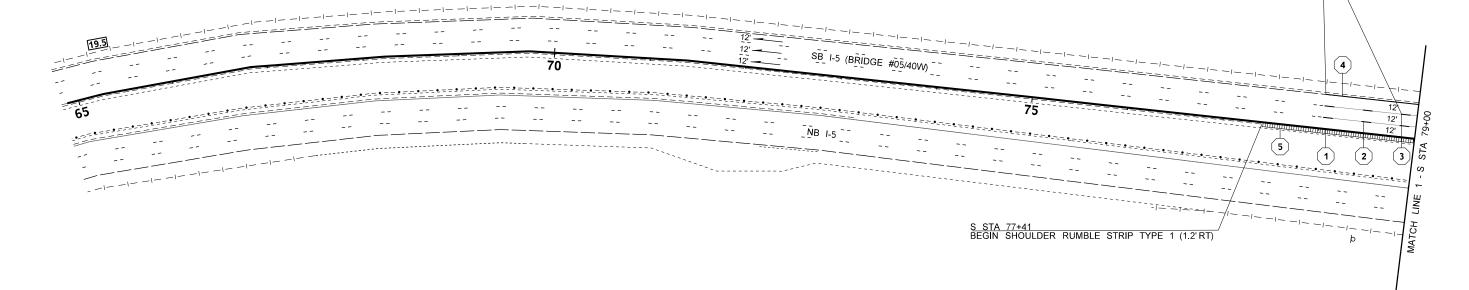
FILE NAME	\\Wsdot.loc\sw\Group\Englnee	ring\444301\05-Design\02-Projects\I-5 NB & SB Lewis R	lver Br and	NB To	outle River Br_I	Deck Repair\1-CADD-Plans_Co	rtractPlans\038-043_22X333_PS_M	K.dgn			Plot 2
TIME	9:19:08 AM				REGION STATE	FED.AID PROJ.NO.	ميدوود.			I-5	PLAN REF NO
DATE	3/14/2023				10 WASH	NHFP-0051(316)	WASKI				MK2
PLOTTED BY	hillb				I IU WASH					NB AND SB LEWIS RVR BR	1411.72
DESIGNED BY	T. PANKAU				JOB NUMBER	1			Washington State	DECK REPAIR / OVERLAY	SHEET
ENTERED BY	CCEO				22X333		White Market State Co.		3		40
CHECKED BY	A. ROMANCHOCK				CONTRACT NO.	LOCATION NO.	36748 PEGISTERED		Department of Transportation		_ OF
PROJ. ENGR.	S. FELL					XL6379	SEE SHEET CT1	DATE	-	PAVEMENT MARKING PLAN	84 SHEETS
REGIONAL ADM	. C. FRANCIS	REVISION	DATE	BY		XE0313	P.E. STAMP BOX	P.E. STAMP BOX			STILLIS

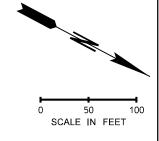




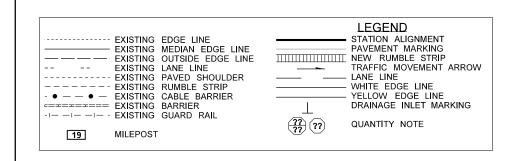
S STA 78+88
MP 19.76
BEGIN PAINT LINE (0'& 36'LT)
BEGIN RAISED PAVEMENT MARKER TYPE 2 (0'& 36'LT)

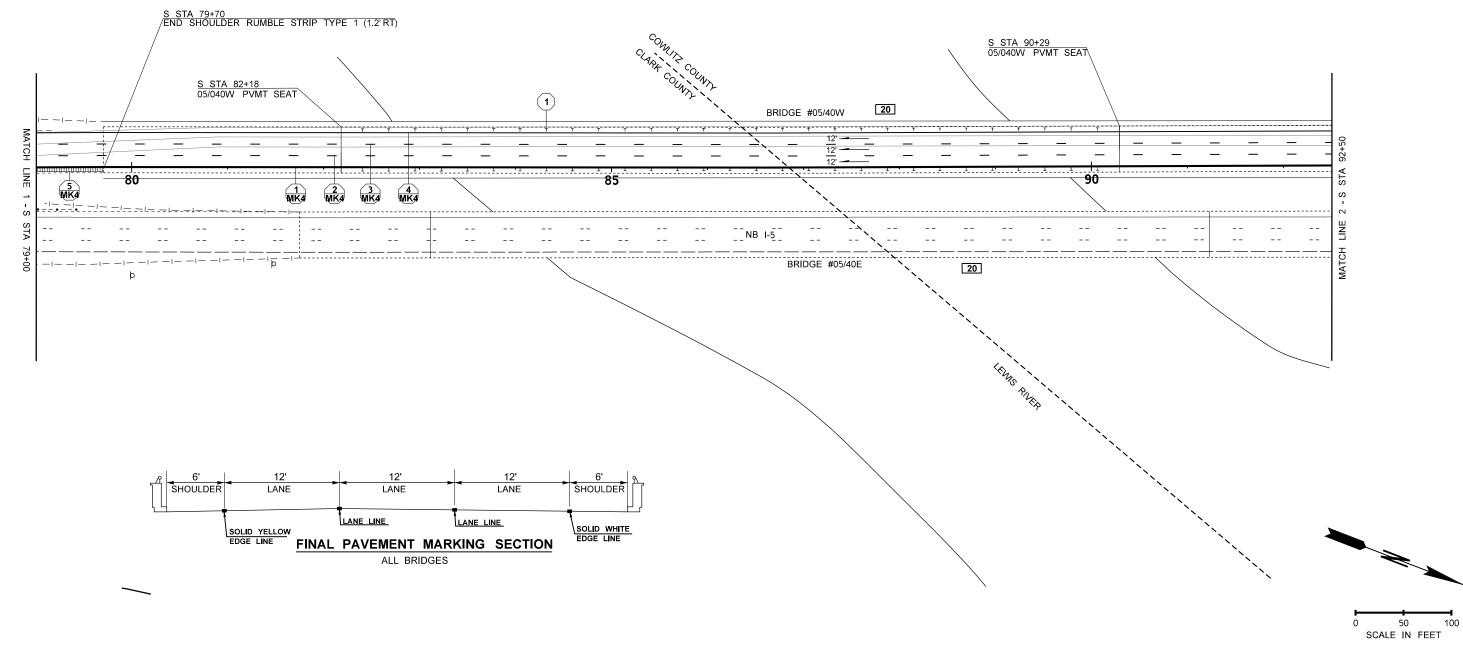
S STA 78+04
MP 19.74
BEGIN REMOVING TEMPORARY PAVEMENT MARKINGS FROM STAGE B
BEGIN PAINT LINE (12'& 24'LT)
BEGIN RAISED PAVEMENT MARKER TYPE 2 (12'& 24'LT)



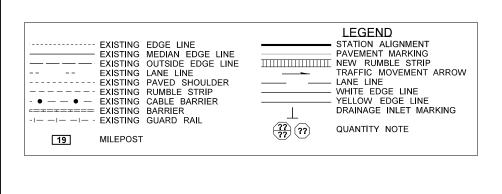


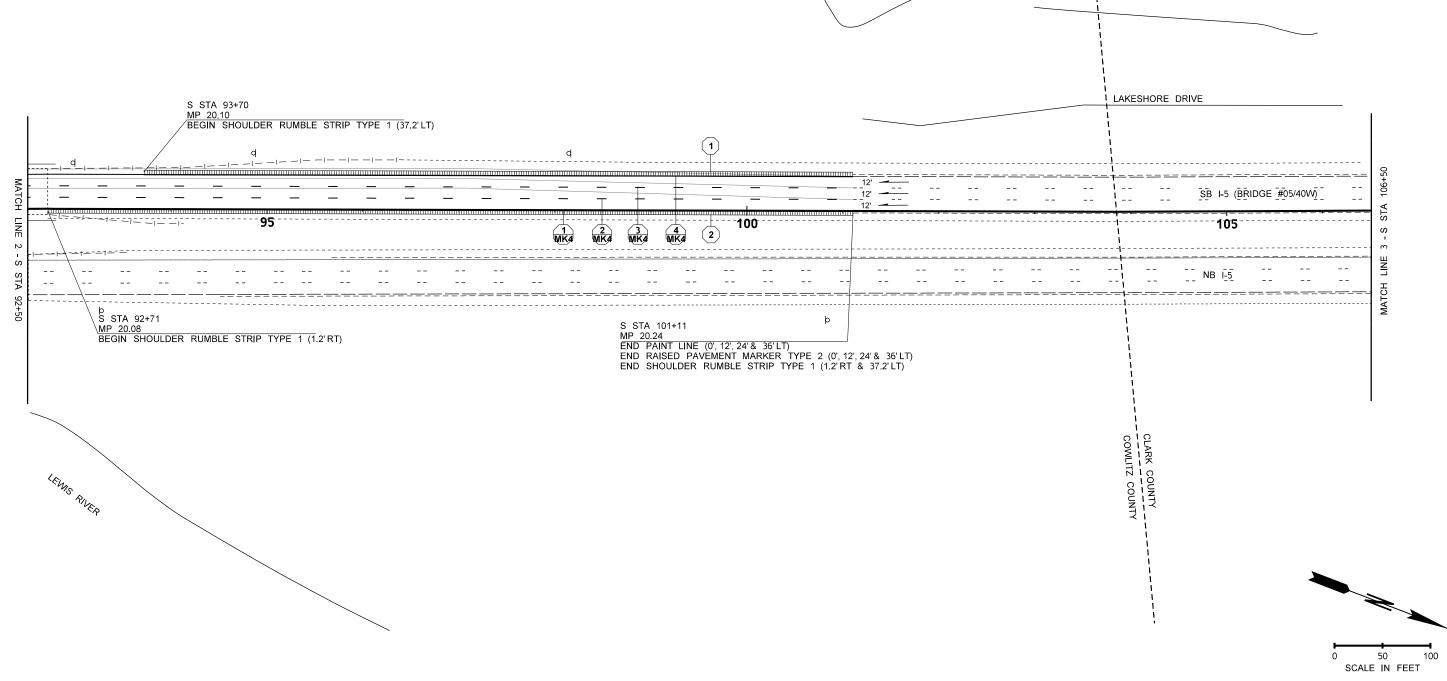
FILE NAME	\\Wsdot.loc\sw\Group\EngIneerIng\444301\05-DesIgn\02-Projects\I-5 NB & S	B Lewis River Br and NB Toutle	River Br_De	ck Repair\1-CADD-Plans_Cor	tractPlans\038-043_22X333_PS_MK.	dgn			Plot 11
TIME	9:19:10 AM	REG	GION STATE	FED.AID PROJ.NO.	مينينين.			I-5	PLAN REF NO
DATE	3/14/2023	1	10 WASH	NHFP-0051(316)	OF WASH				MK4
PLOTTED BY	hillb	'	WASH	,				NB AND SB LEWIS RVR BR	""\-
DESIGNED BY	T. PANKAU		JOB NUMBER		A Control of the second of the		Washington State	DECK REPAIR / OVERLAY	SHEET
ENTERED BY	CCEO	22	2X333		WWW.			DECK KEFAIK / OVERLAT	42
CHECKED BY	A. ROMANCHOCK	C	CONTRACT NO.	LOCATION NO.	PEGISTERED S		Department of Transportation		OF
PROJ. ENGR.	S. FELL			XL6380	SEE SHEET CT1	DATE		PAVEMENT MARKING PLAN	84 SHEETS
REGIONAL ADM.	C. FRANCIS REVISION	DATE BY		ALOJOU	P.E. STAMP BOX	P.E. STAMP BOX			SHEETS





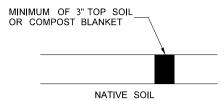
FILE NAME	\\Wsdot.loc\sw\Group\EnglneerIn	ig\444301\05-Design\02-Projects\I-5 NB & SB Lewis	River Br and	NB To	utle River Br_C	Deck Repair\1-CADD-Plans_Co	rtractPlans\038-043_22X333_PS_MK	dgn			Plot 12
TIME	9:19:10 AM				REGION STATE	FED.AID PROJ.NO.	apparage.			I-5	PLAN REF N
DATE	3/14/2023				10 WASH	NHFP-0051(316)	SE WASH				MK5
PLOTTED BY	hillb				IU WASH					NB AND SB LEWIS RVR BR	141113
DESIGNED BY	T. PANKAU				JOB NUMBER				Washington State	DECK REPAIR / OVERLAY	SHEET
ENTERED BY	CCEO				22X333		Munitipe Contract		J 3 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1	DECK KLFAIK / OVLKLAT	43
CHECKED BY	A. ROMANCHOCK				CONTRACT NO.	LOCATION NO.	36748 PEGISTERED		Department of Transportation		OF
PROJ. ENGR.	S. FELL					XL6380	SEE SHEET CT1	DATE		PAVEMENT MARKING PLAN	84 SHEETS
REGIONAL ADM.	I. C. FRANCIS	REVISION	DATE	BY		ALOJOU	P.E. STAMP BOX	P.E. STAMP BOX			SHEETS





HORSESHOE LAKE

FILE NAME	\\Wsdot.loc\sw\Group\EngIneerIng\444301\05-DesIgn\02-Projects\I-5 NB & S	B Lewis River Brand NB	Toutle River Br_I	Deck Repair\1-CADD-Plans_Co	ortractPlans\038-043_22X333_PS_MK.	dgn			Plot 13
TIME	9:19:11 AM		REGION STATE	FED.AID PROJ.NO.	معموم			I-5	PLAN REF NO
DATE	3/14/2023		10 WASH	NHFP-0051(316)	OF WASHI				MK6
PLOTTED BY	hillb		IU WASH					NB AND SB LEWIS RVR BR	"""
DESIGNED BY	T. PANKAU		JOB NUMBER		Visit Day Bone Fell		Washington State	DECK REPAIR / OVERLAY	SHEET
ENTERED BY	CCEO		22X333					DECK KEFAIK / OVERLAT	44
CHECKED BY	A. ROMANCHOCK		CONTRACT NO.	LOCATION NO.	36748 PEGISTERES		Department of Transportation		OF
PROJ. ENGR.	S. FELL			XL6380	SEE SHEET CT1	DATE		PAVEMENT MARKING PLAN	84 SHEETS
REGIONAL ADM.	C. FRANCIS REVISION	DATE B	BY	XE0300	P.E. STAMP BOX	P.E. STAMP BOX			SHEETS



SEEDING, FERTILIZING AND MULCHING

REVISION

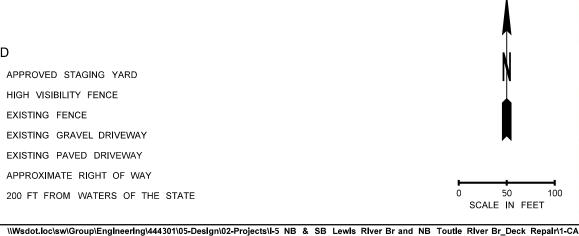
NOTES

FILE NAME

REGIONAL ADM. C. FRANCIS

- 1. EXISTING PAVED / GRAVEL DRIVEWAY TO LEWIS RIVER SHALL BE UNOBSTRUCTED AT ALL TIMES AND AVAILABLE FOR STATE AGENCIES TO ACCESS LEWIS RIVER.
- 2. NO STAGING SHALL BE PERMITTED OUTSIDE OF WSDOT RIGHT OF WAY, OUTSIDE HATCHED AREAS, OR BEHIND HIGH VISIBILITY FENCE UNLESS APPROVED BY THE
- 3. NO STAGING SHALL BE PERMITTED UNDER THE SOUTH APPROACH TO EITHER BRIDGE.
- 4. NO CONSTRUCTION VEHICLE ACCESS SHALL BE PERMITTED FROM THE STAGING YARD DIRECTLY TO 1-5.
- 5. NO TRESPASSING SIGNS SHALL BE INSTALLED AS STAKED BY THE ENGINEER.
- 6. STAGING YARD SHALL BE RETURNED TO ORIGINAL CONDITIONS AT COMPLETION OF PROJECT. IF CONTRACTOR ELECTS TO INSTALL GRAVEL IN THE STAGING YARD, THE CONTRACTOR SHALL INSTALL WATTLES ON THE DOWNHILL SLOPE AS APPROVED BY THE ENGINEER INSTALLATION AND REMOVAL OF BOTH ITEMS SHALL BE AT NO COST TO THE CONTRACTING AGENCY, SEE SPECIAL PROVISION "SEEDING, FERTILIZING AND
- 7. SEE SPECIAL PROVISION ENVIRONMENTAL REGULATIONS.

LEGENI	D
	APPROVED STAGING YARD
- HVF	HIGH VISIBILITY FENCE
-xx-	EXISTING FENCE
	EXISTING GRAVEL DRIVEWAY
	EXISTING PAVED DRIVEWAY
	APPROXIMATE RIGHT OF WAY
	200 FT FROM WATERS OF THE ST



10 WASH

JOB NUMBER

22X333

DATE

BY

PINKERTON DRIVE			PIER 5	
100 I FEET Book Repair\1-CADD-Plans_Cor tractPlans\044_22X333_PS_SUEC.d		PROXIMATE TOP OF RIVER BANK	PIER 4 BRIDGE 05/40W	Plot 1
FED.AID PROJ.NO.	-		I-5	PLAN REF NO

TIME	9:50:08 AM	
DATE	3/14/2023	
PLOTTED BY	hillb	
DESIGNED BY	T. PANKAU	
ENTERED BY	CCEO	
CHECKED BY	A. ROMANCHOCK	
PROJ. ENGR.	S. FELL	

FED.AID PROJ.NO. NHFP-0051(316)	Ann West confield
LOCATION NO.	36748
XL6379 & XL6380	SEE SHEET CT1 DATE
	P.E. STAMP BOX

Washington State **Department of Transportation**

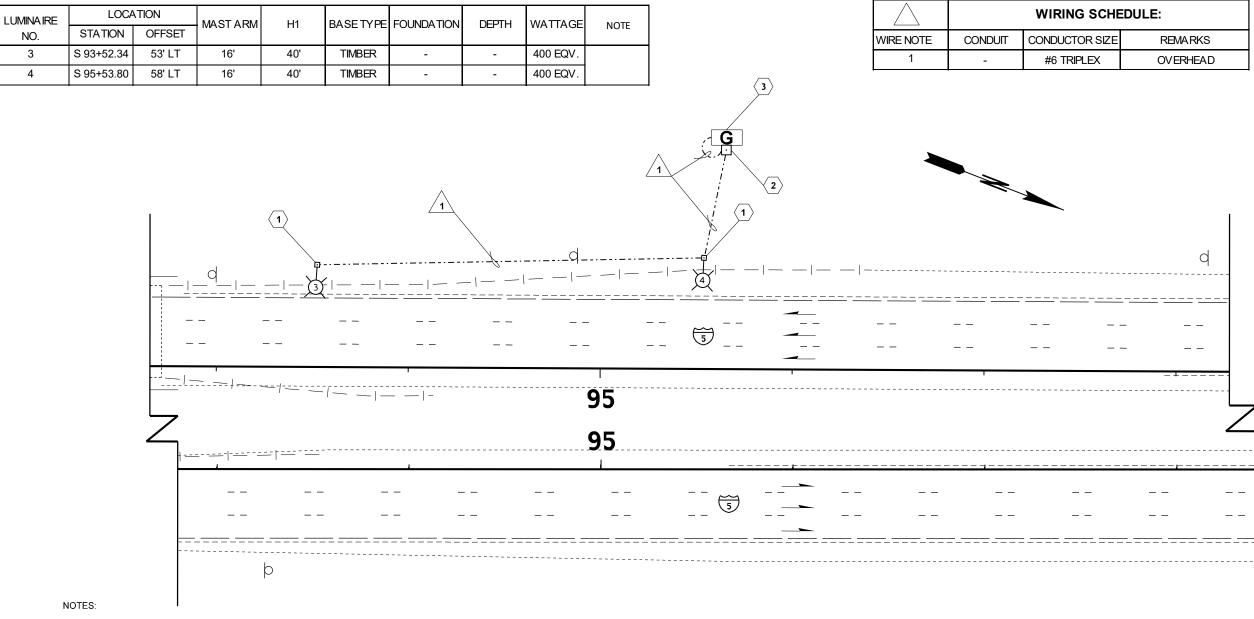
DATE

NB AND SB LEWIS RVR BR DECK REPAIR / OVERLAY

STAGING TESC PLAN

SUEC1 84

		-	TEMPO	RARYIIIII	MINATION SYSTE	M					NI NOTEO
	LOCAT		LIVIEUE	Taki illul		- 				CONSTRUCTION	
LUMINA IRE NO.	STATION	OFFSET	MASTARM	M H1	BASE TYPE FOUND	DATION DEPTH	WATTAGE	NOTE	1 1 1	LL A TEMPORARY 50 FT TIMBER POLE, 16 FT MAS OMPONENTS PER STANDARD PLAN J-15.10 FOR A	STARM, 400W OR EQV.LUMINAIRE, GUY WIRE, ANCHOR ANI TEMPORARY TIMBER LIGHT STANDARD
1	N 79+00.23	55' RT	16'	40'	TIMBER -		400 EQV.		FURNISH AND INSTA		RY TIMBER POLE SHALL BE INSTALLED PER STANDARD
2	N 80+93.60	53' RT	16'	40'	TIMBER -		400 EQV.		2 SPECIFICATION 8-20.	• • •	
											CE OF POWER FOR A TEMPORARY ILLUMINATION SYSTEM. ABLE TO MAINTAIN LIGHTING LEVELS DURING HOURS OF
			-								DETERMINE THE SELECTED POWER SOURCE WATTAGE, SIZ
					V	WIRING SCHEDU	JLE:		·	· · · · · · · · · · · · · · · · · · ·	, BREAKERS, OR ANY OTHER ACCESSORIES NEEDED TO
			,	WIRE NOTE	CONDUIT CO	NDUCTOR SIZE	REMAR	RKS	POWER THE TEMPOR	RARY ILLUMINATION SYSTEM.	
			Ī	1	-	#6 TRIPLEX	OVERH	EAD			
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						_			\	NOTES:	
					G		<u>1</u>	\sum		1. THE LOCATIONS OF FEATURES SHOWN IN THE P	LANS ARE APPROXIMATE AND SHALL BE VERIFIED IN THE FIELD
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LEGEN	ID			3	G		<u>/1</u>			BY THE CONTRACTOR. 2. TEMPORARY ILLUMINATION SHALL BE INSTALLED INSTALLED.	AND OPERATIONAL PRIOR TO THE CONCRETE BARRIER BEING
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- TEMPORARY ILLUMINATION SHALL BE INSTALLED AND OPERATIONAL PRIOR TO THE TEMPORARY CONCRETE BARRIER
- TEMPORARY ILLUMINATION IS ONLY REQUIRED TO BE ACTIVE AND MAINTAINED WHEN TEMPORARY CONCRETE BARRIER IS IN PLACE.
- CONTRACTOR MAY ELECT TO USE TEMPORARY PORTABLE LIGHT PLANTS IN LIEU OF TEMPORARY TIMBER POLES. IF THE CONTRACTOR ELECTS TO USE TEMPORARY PORTABLE LIGHT PLANTS, THE CONTRACTOR SHALL SUBMIT A TEMPORARY LIGHTING PLAN TO THE TRAFFIC ENGINEER FOR APPROVAL PRIOR TO USE. APPROVAL OF THE TEMPORARY LIGHTING PLAN MAY TAKE UP TO 5 WORKING DAYS PER SUBMITTAL/REVISION. TEMPORARY LIGHTING AND APPROVAL SHALL BE INCIDENTAL TO THE BID ITEM "TEMPORARY LILUMINATION SYSTEM". CONTRACTOR SHALL REFER TO WSDOT 2022 DESIGN MANUAL CHAPTER 1040 FOR LIGHTING REQUIREMENTS. TEMPORARY LIGHTING PLAN SHALL INCLUDE, BUT NOT BE LIMITED TO THE LOCATION(S) OF THE TEMPORARY LIGHT PLANTS, LIGHTING LEVEL CALCULATIONS AND LAYOUT REFER TO WSDOT DESIGN MANUAL SECTION 1040.06, EXHIBIT 1040-39 AND 1040-45
- DURING HOURS OF DARKNESS AND/OR INCLEMENT WEATHER THE CONTRACTOR SHALL MAINTAIN LIGHTING LEVELS THROUGHOUT THE PROJECT. HOURS OF DARKNESS ARE DEFINED AS 1/2 HOUR PRIOR TO SUNSET AND 1/2 HOUR AFTER SUN RISE
- ALL ITEMS ON THIS SHEET, INCLUDING THE REMOVAL OF THE TEMPORARY ILLUMINATION SYSTEM, PUD REQUIREMENTS, BILLING OR ANY OTHER ASSOCIATED MATERIALS NEEDED TO PROVIDE TEMPORARY ILLUMINATION SHALL BE INCIDENTAL TO THE BID ITEM "TEMPORARY ILLUMINATION SYSTEM".

3

	7. REFER TO PLAN SH	HEET TIL1 FOR LEGEND.						
FILE NAME	G:\Traffic\TRAFFIC DESIGN\0	1-ACTIVE DESIGN FILES\XL6378_I-5 Bridge Overlay\Bas	se\XL6378_PS	_TIL.d	gn			Т
TIME	10:19:01 AM				REGION NO.	STATE	FED.AID PROJ.NO.	1
DATE	4/25/2023				10	WASH	NHFP-0051(316)	اوا
PLOTTED BY	wachtek				10	WASH		13
DESIGNED BY	K. WACHTER					IUMBER		1
ENTERED BY	K. WACHTER				22X	.333		3
CHECKED BY	SWR TRAFFIC				CONTR	RACT NO.	LOCATION NO.	1
PROJ. ENGR.	L. SCHILPEROORT						XL6379	1
REGIONAL ADM.	C. FRANCIS	REVISION	DATE	BY			AE0313	

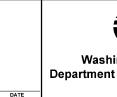


SCALE IN FEET

2

SPECIFICATION 8-20.3(13)A

POWER THE TEMPORARY ILLUMINATION SYSTEM.



Washington State
Department of Transportation

ALL NECESSARY COMPONENTS PER STANDARD PLAN J-15.10 FOR A TEMPORARY TIMBER LIGHT STANDARD.

CONSTRUCTION NOTES:

FURNISH AND INSTALL A TEMPORARY 50 FT TIMBER POLE, 16 FT MAST ARM, 400W OR EQV.LUMINAIRE, GUY WIRE, ANCHOR AND

FURNISH AND INSTALL A 20 FT TEMPORARY TIMBER POLE. TEMPORARY TIMBER POLE SHALL BE INSTALLED PER STANDARD.

FURNISH AND INSTALL A TEMPORARY GENERATOR OR OTHER SOURCE OF POWER FOR A TEMPORARY ILLUMINATION SYSTEM. GENERATOR OR OTHER SOURCE OF TEMPORARY POWER SHALL BE ABLE TO MAINTAIN LIGHTING LEVELS DURING HOURS OF DARKNESS AND/ OR INCLEMENT WEATHER. THE CONTRACTOR SHALL DETERMINE THE SELECTED POWER SOURCE WATTAGE, SIZE,

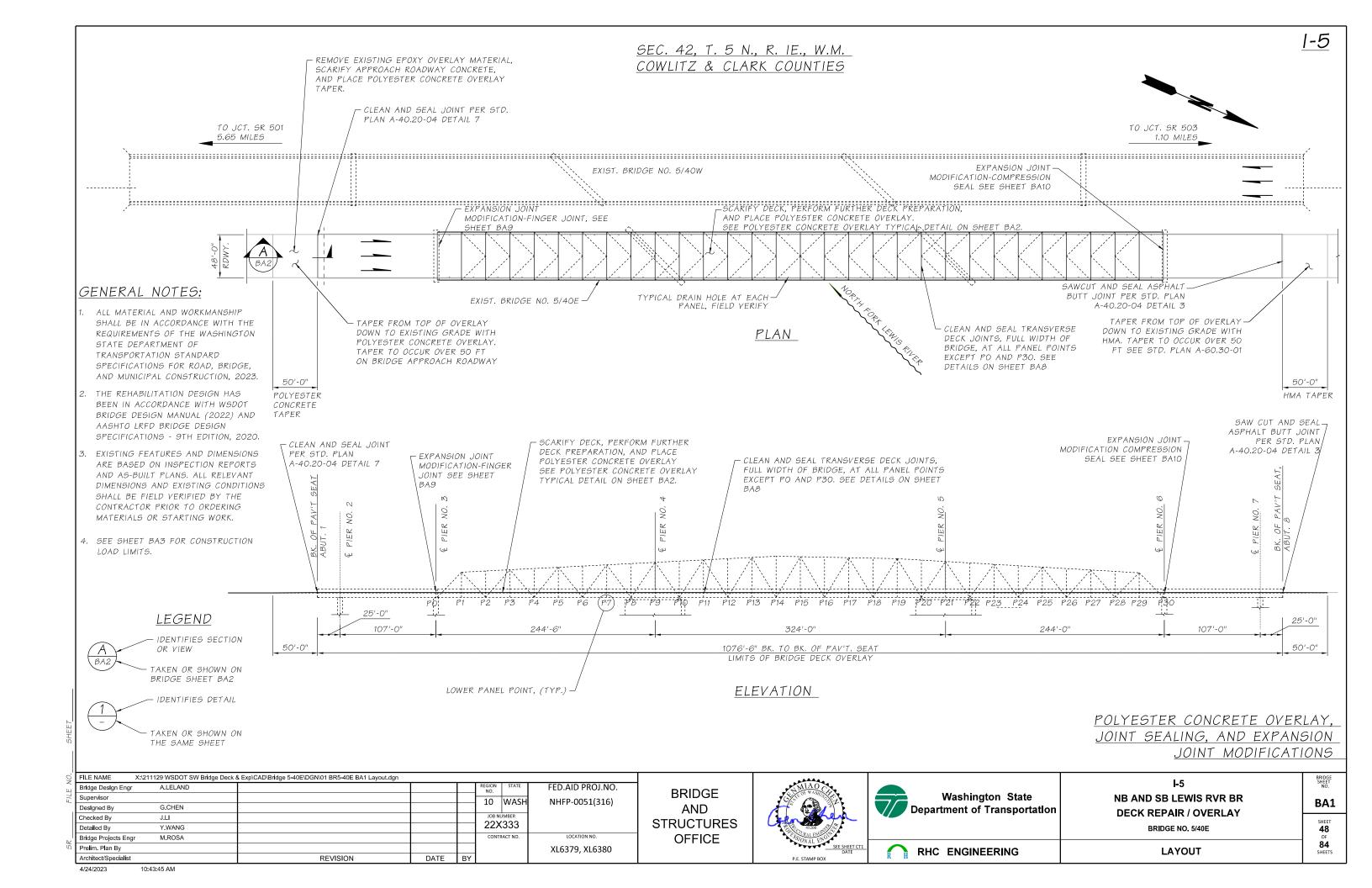
LOCATION, AS WELL AS ANY OTHER ADDITIONAL WIRES, ADAPTERS, BREAKERS, OR ANY OTHER ACCESSORIES NEEDED TO

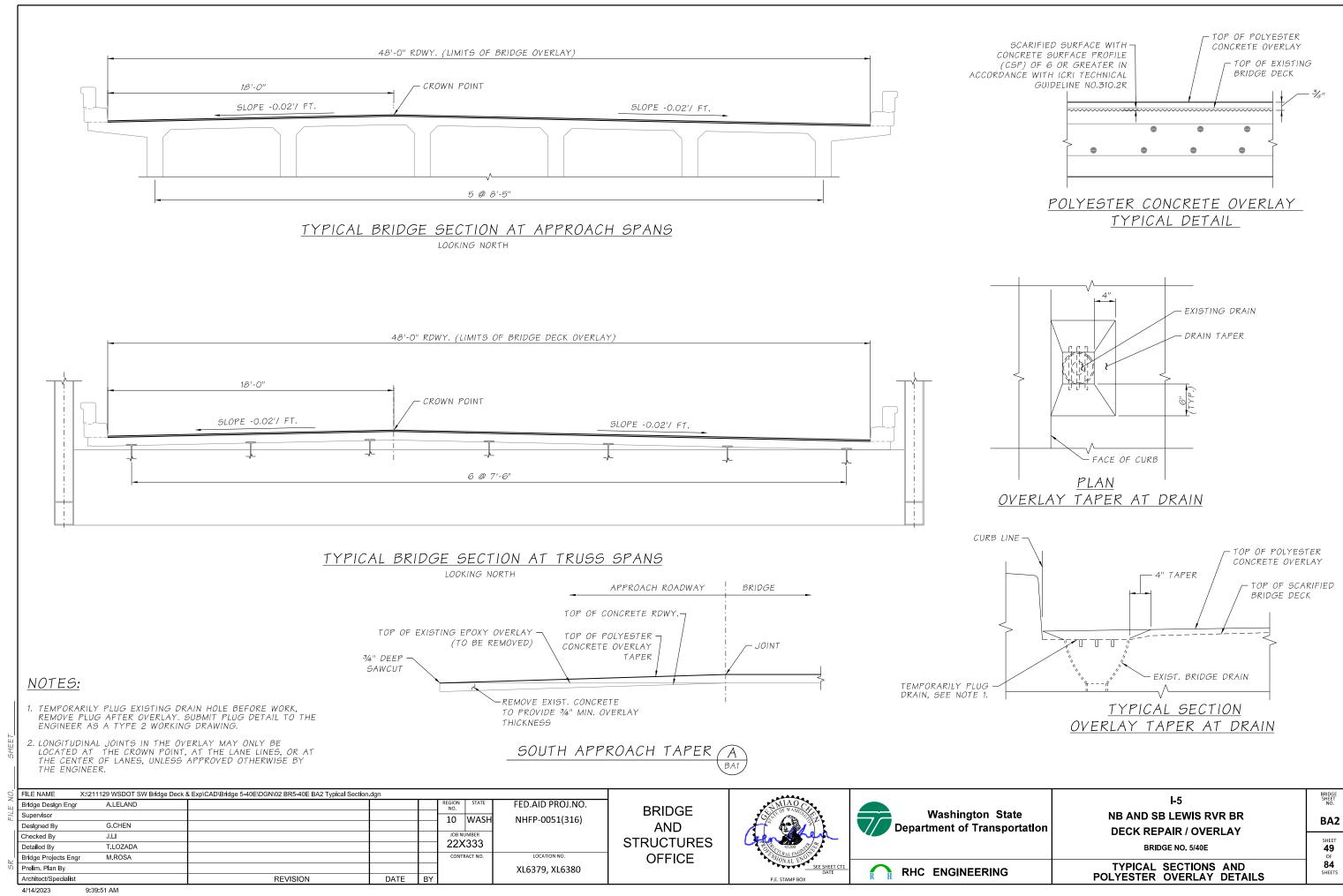
I-5 NB AND SB LEWIS R BR. **DECK REPAIR / OVERLAY**

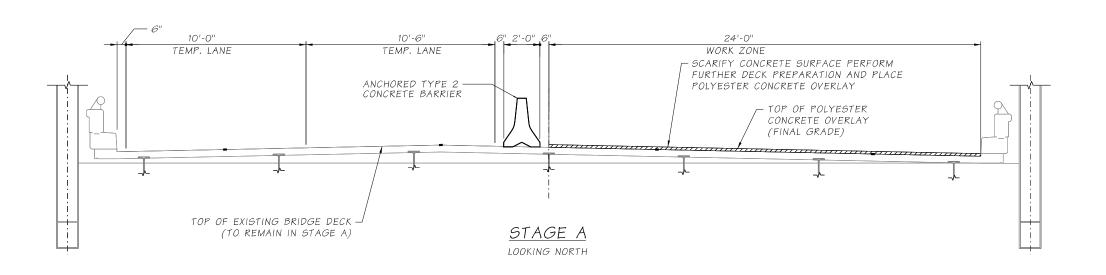
TEMPORARY ILLUMINATION SYSTEM

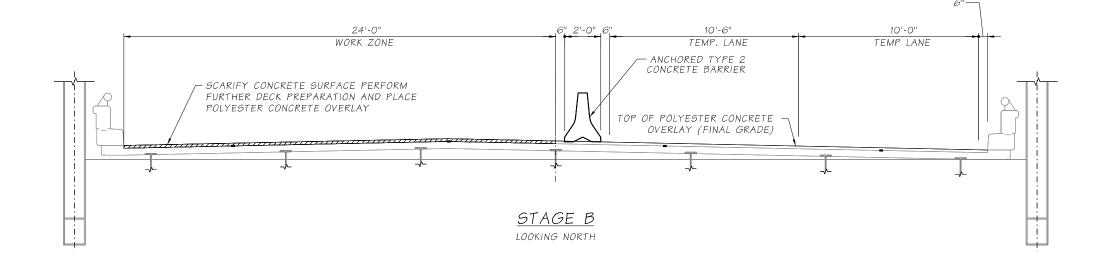
Plot 2 PLAN REF N TIL2

84









CONSTRUCTION LOAD LIMITS:

- 1. TRAFFIC SHALL BE CONFIGURED AS SHOWN ON THIS PLAN SHEET.
- 2. ONLY ONE LINE OF TEMPORARY CONCRETE BARRIER IS ALLOWED ON THE BRIDGE. THE WEIGHT OF THE TEMPORARY CONCRETE BARRIER SHALL NOT EXCEED 410 PLF.
- 3. THE MAXIMUM TOTAL CONSTRUCTION LOAD ON THE BRIDGE, INCLUDING EQUIPMENT AND MATERIALS (EXCEPT FOR THE TEMPORARY CONCRETE BARRIER), SHALL NOT EXCEED 81,000 LBS
- 4. THE MAXIMUM TOTAL CONSTRUCTION LOAD TRIBUTARY TO ANY OF THE 62 LOWER PANEL POINTS, INCLUDING EQUIPMENT AND MATERIALS (EXCEPT FOR THE TEMPORARY CONCRETE BARRIER), SHALL NOT EXCEED 40,500 LBS.
- 5. WORKING DRAWINGS AND CALCULATIONS (TYPE 2E) SHALL BE SUBMITTED IN ACCORDANCE WITH STANDARD SPECIFICATION SECTION 1-05.3. WORKING DRAWINGS AND CALCULATIONS SHALL SHOW THAT ALL OF THE ABOVE CONSTRAINTS HAVE BEEN MET FOR ALL CONSTRUCTION LOADS. CONSTRUCTION LOADS SHALL NOT BE PLACED ON THE BRIDGE UNTIL THE ASSOCIATED WORKING DRAWINGS AND CALCULATIONS HAVE BEEN REVIEWED AND ACCEPTED BY WSDOT.
- 6. REMOVED MATERIAL, DEBRIS, AND SPENT ABRASIVE BLAST MEDIA SHALL BE COLLECTED AND REMOVED FROM THE BRIDGE DAILY, AT A MINIMUM, TO KEEP UNNECESSARY WEIGHT OFF THE BRIDGE.

Š.,	FILE NAME X:\211	1129 WSDOT SW Bridge Deck	& Exp\CAD\Bridge 5-40E\DGN\03 BR5-40E BA3 Construction Se	quence.dgn				
~	Bridge Design Engr	A.LELAND				REGION NO.	STATE	FED.AID PROJ.NO.
7	Supervisor					10	WASH	NHFP-0051(316)
	Designed By	G.CHEN						14111 0031(310)
	Checked By	J.LI				JOB NI		
	Detailed By	T.LOZADA				22X	(333	
V	Bridge Projects Engr	M.ROSA				CONTI	RACT NO.	LOCATION NO.
Ñ	Prelim. Plan By							XL6379, XL6380
	Architect/Specialist		REVISION	DATE	BY			

BRIDGE
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OFFICE





RHC ENGINEERING

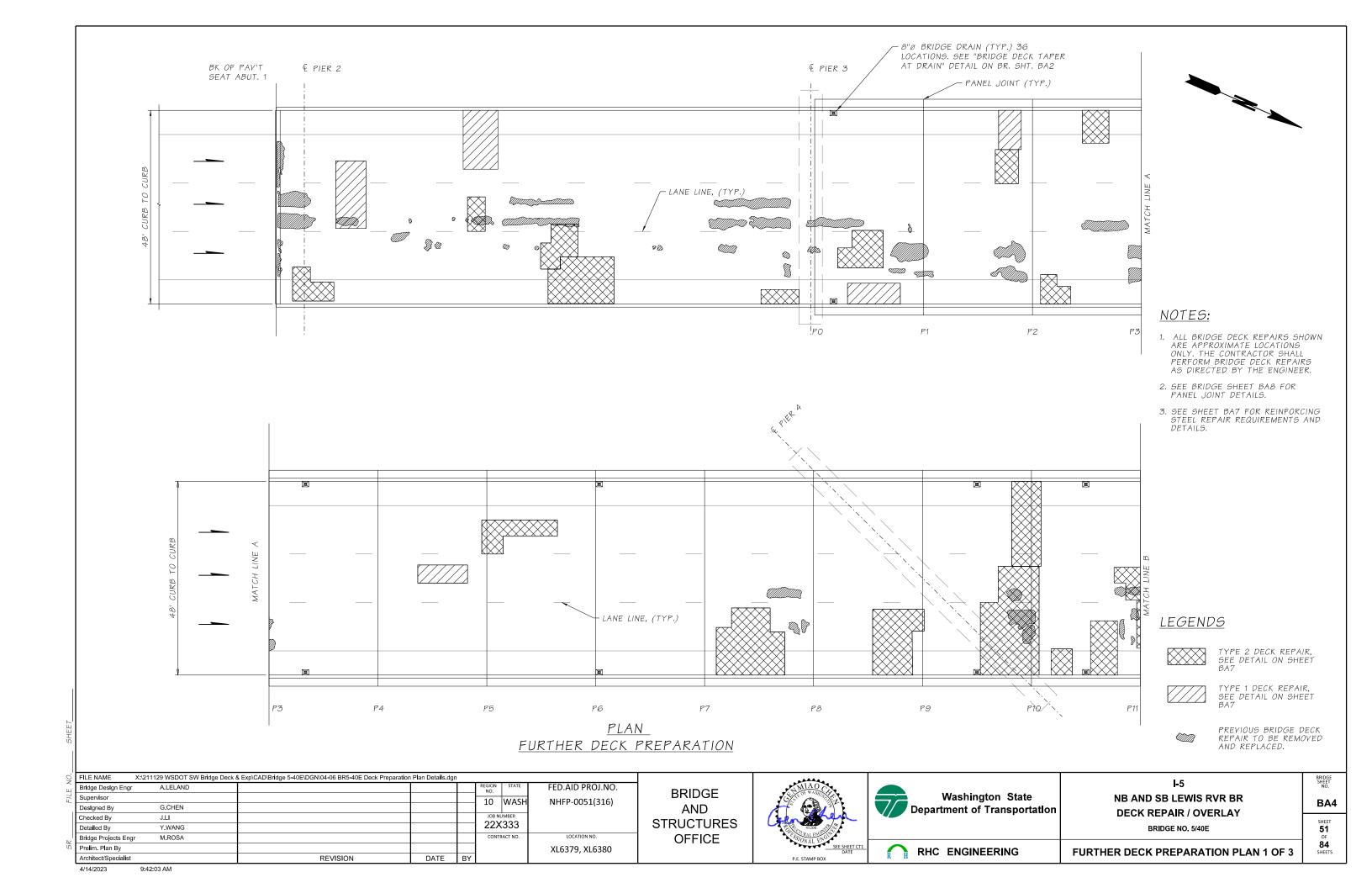
I-5 NB AND SB LEWIS RVR BR DECK REPAIR / OVERLAY

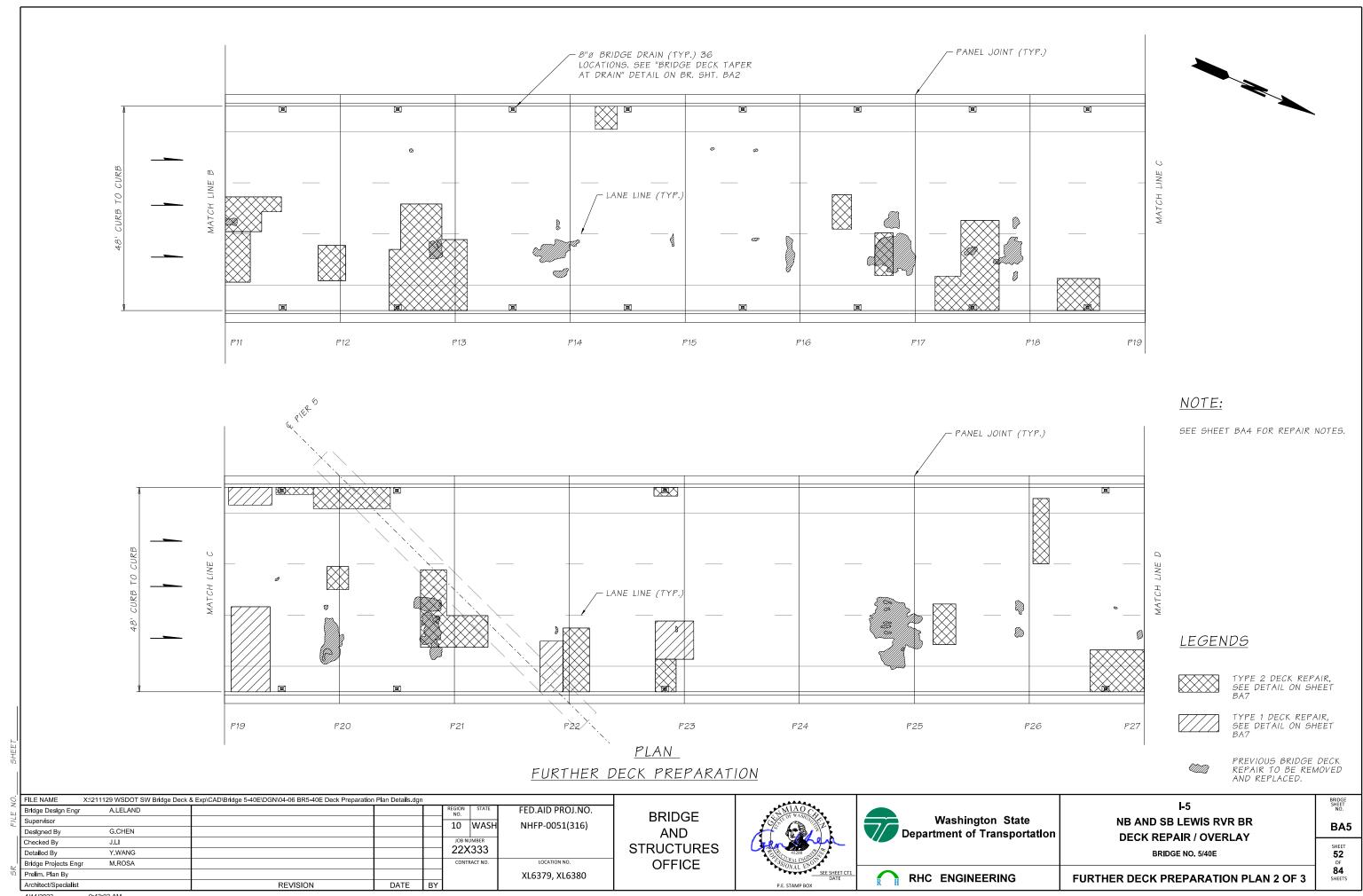
BRIDGE NO. 5/40E

CONSTRUCTION STAGING AND LOAD LIMITS

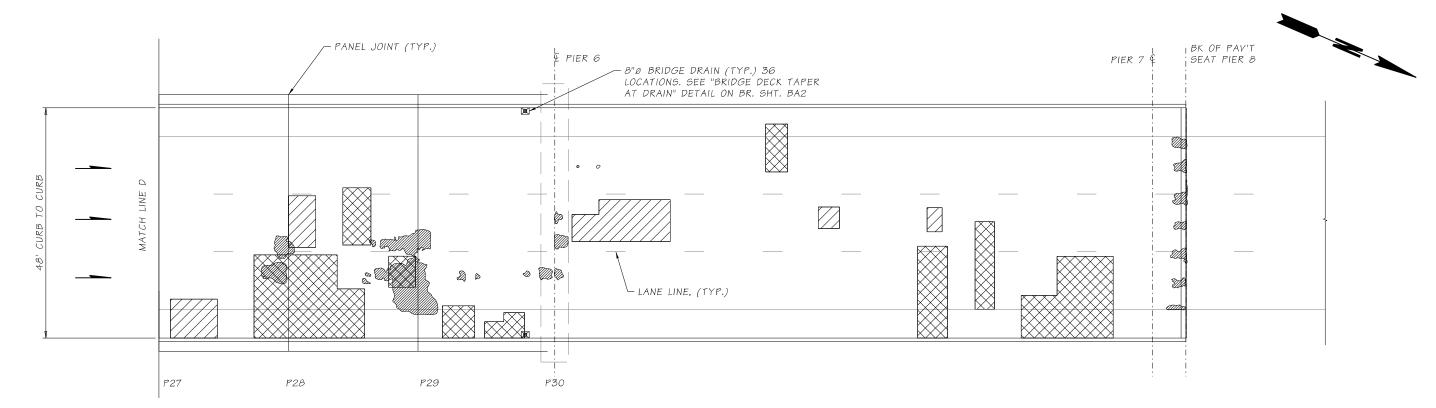


NO. 5/40E





4/14/2023 9:43:03 AM



PLAN FURTHER DECK PREPARATION

NOTE:

SEE SHEET BA4 FOR REPAIR NOTES.

<u>LEGENDS</u>



TYPE 2 DECK REPAIR, SEE DETAIL ON SHEET BA7



TYPE 1 DECK REPAIR, SEE DETAIL ON SHEET BA7



PREVIOUS BRIDGE DECK REPAIR TO BE REMOVED AND REPLACED.

NO.	FILE NAME	X:\211129 WSDOT SW Bridge Deck	& Exp\CAD\Bridge 5-40E\DGN\04-06 BR5-40E Deck Preparation	ı Plan Details.dgr	ı				
Ш	Bridge Design Engr	A.LELAND				REGION NO.	STATE	FED.AID PROJ.NO.	
FIL	Supervisor					10	WASH	NHFP-0051(316)	
	Designed By	G. CHEN				10	WASII	11111-0031(310)	
	Checked By	J.LI				JOB NU			
	Detailed By	Y.WANG				22X	333		
ΩZ.	Bridge Projects Engr	M.ROSA				CONTR.	ACT NO.	LOCATION NO.	
Ś	Prelim. Plan By					1		XL6379, XL6380	
	Architect/Specialist		REVISION	DATE	BY			ŕ	

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I-5 NB AND SB LEWIS RVR BR **DECK REPAIR / OVERLAY**

BRIDGE SHEET NO.

BA6

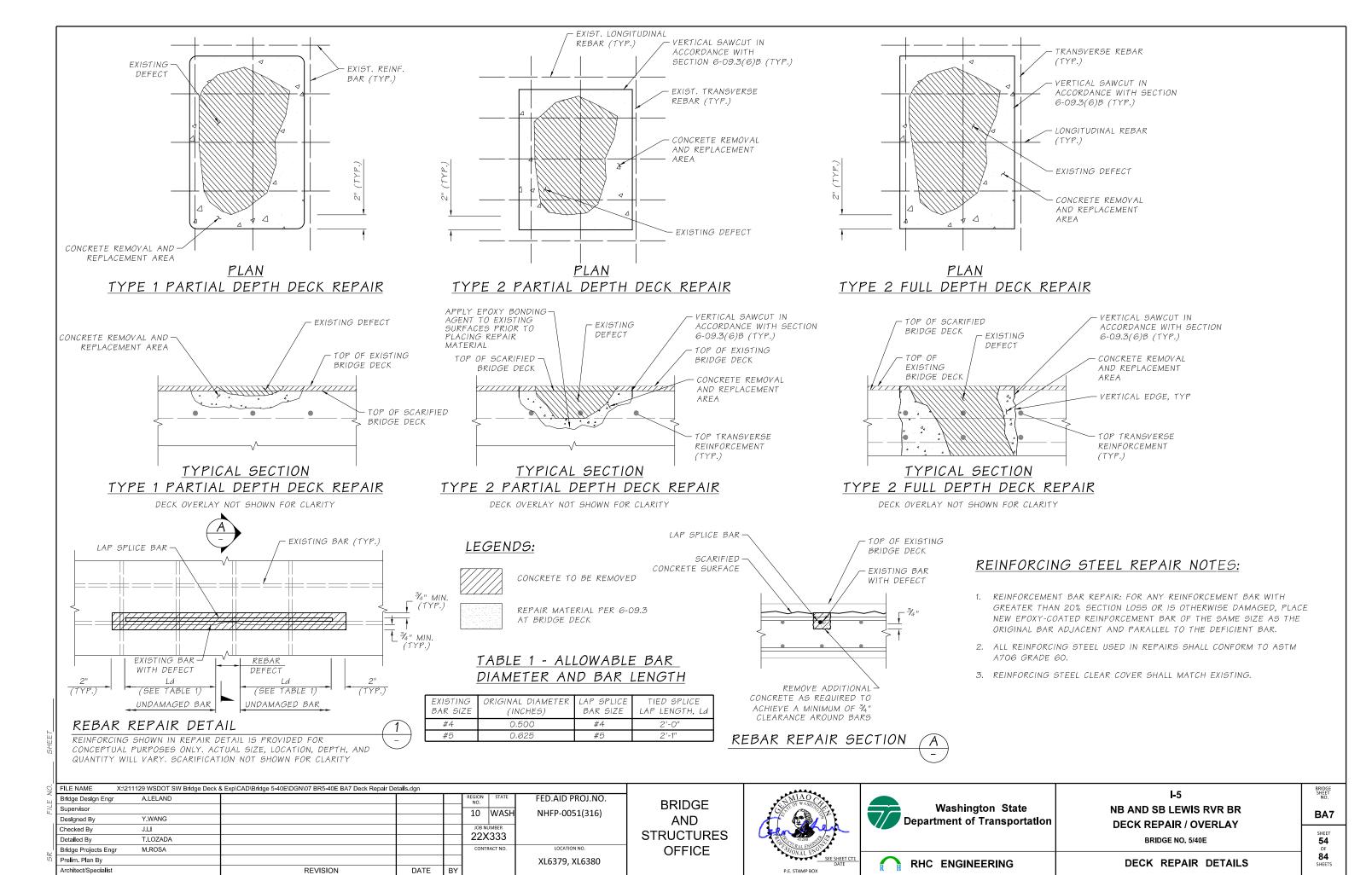
SHEET 53

84 SHEETS

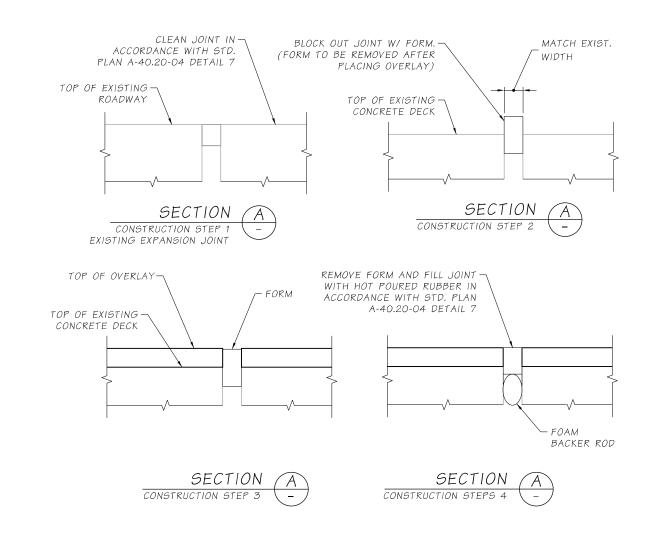
BRIDGE NO. 5/40E

RHC ENGINEERING

FURTHER DECK PREPARATION PLAN 3 OF 3



4/14/2023 9:44:32 AM



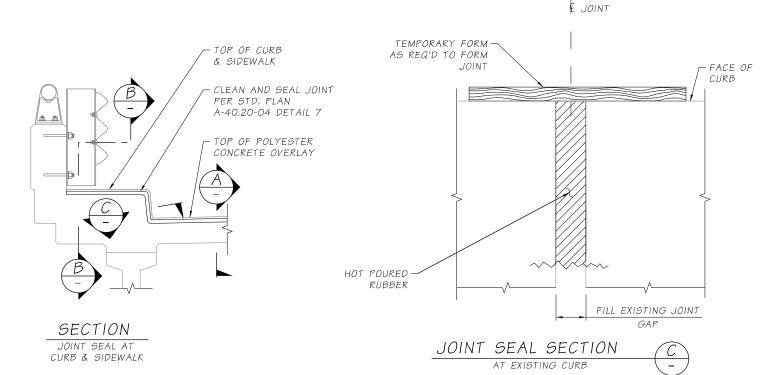
<u>SUGGESTED CONSTRUCTION</u> <u>SEQUENCE:</u>

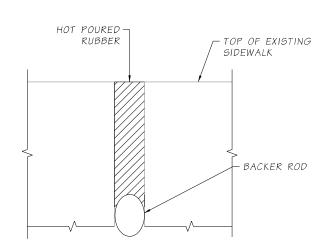
STEP 1. CLEAN JOINT

STEP 2. PLACE FORM IN EXISTING JOINT OPENING.

STEP 3. PLACE POLYESTER CONCRETE OVERLAY TO FINAL RDWY. ELEVATION.

STEP 4. REMOVE FORM FROM JOINT OPENING AND FILL WITH HOT POURED RUBBER.





JOINT SEAL DETAIL B

Ò.	FILE NAME X:1211129 WSDOT SW Bridge Deck & Exp\CAD\Bridge 5-40E\DGN\08 BR5-40E BA8 Seal Joint Modification Details.dgn								
~	Bridge Design Engr	A.LELAND				REGION NO.	STATE	FED.AID PROJ.NO.	
7	Supervisor					10	WASH	NHFP-0051(316)	
	Designed By	G.CHEN				10	VVASII	141111 0031(310)	
	Checked By	J.LI					UMBER		ے ا
	Detailed By	T.LOZADA				22)	(333		٦
\ \	Bridge Projects Engr	M.ROSA				CONT	RACT NO.	LOCATION NO.	
Ñ	Prelim. Plan By							XL6379, XL6380	
	Architect/Specialist		REVISION	DATE	BY			,	

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I-5 NB AND SB LEWIS RVR BR DECK REPAIR / OVERLAY

BRIDGE NO. 5/40E

RHC ENGINEERING DECK JOINT SEALANT DETAIL

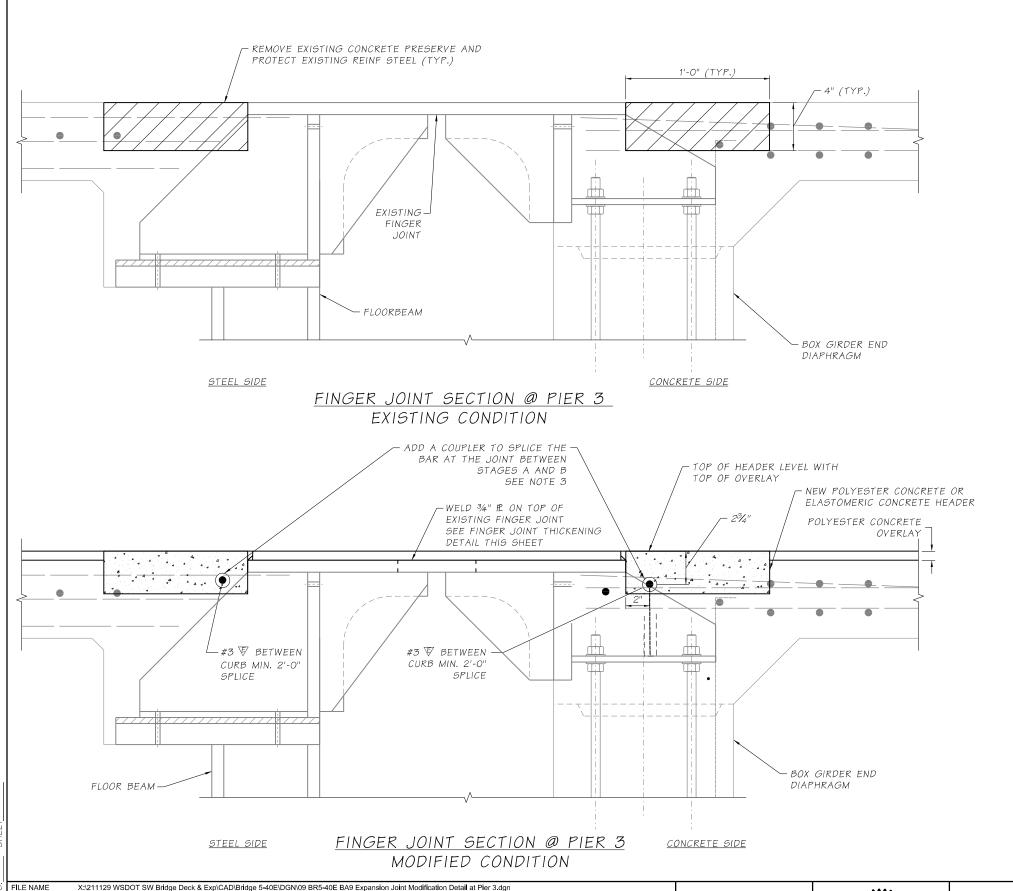
SHEET

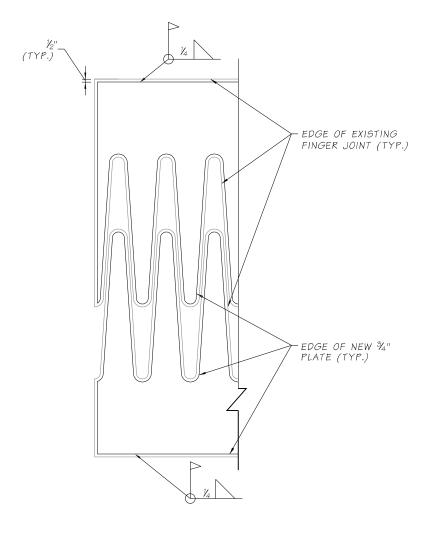
4/14/2023 9:45:42 AM

BRIDGE SHEET NO.

> SHEET 55

> 84





FINGER JOINT THICKENING DETAIL

NOTES:

- 1. NEW STEEL SHALL BE ASTM A573 OR A36.
- 2. ALL EXISTING STEEL REINFORCEMENT AND STRUCTURAL STEEL SHALL BE PRESERVED. NOTIFY ENGINEER FOR ANY OBSERVED DEFECTS.
- 3. THE COUPLER SHALL BE CAPABLE OF DEVELOPING THE FULL STRENGTH OF THE BAR. THERE SHALL BE A MINIMUM OF 2" OF CONCRETE COVER OVER THE COUPLER.

Š.	FILE NAME X:\21	1129 WSDOT SW Bridge Deck	Modification Del	etail at Pier 3.dgn					
~	Brldge Design Engr	A.LELAND				REGION NO.	STATE	FED.AID PROJ.NO.	
FIL	Supervisor					10	WASH	NHFP-0051(316)	
	Designed By	G.CHEN				10	VVASII	141111 0031(310)	
	Checked By	J.LI				JOB NUMBER 22X333			
	Detailed By	T.LOZADA							
ΩZ	Bridge Projects Engr	M.ROSA				CONTR	RACT NO.	LOCATION NO.	
Ō	Prelim. Plan By							XL6379, XL6380	
	Architect/Specialist		REVISION	DATE	BY			•	

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	Washington State
	Department of Transportation

I-5 **NB AND SB LEWIS RVR BR DECK REPAIR / OVERLAY**

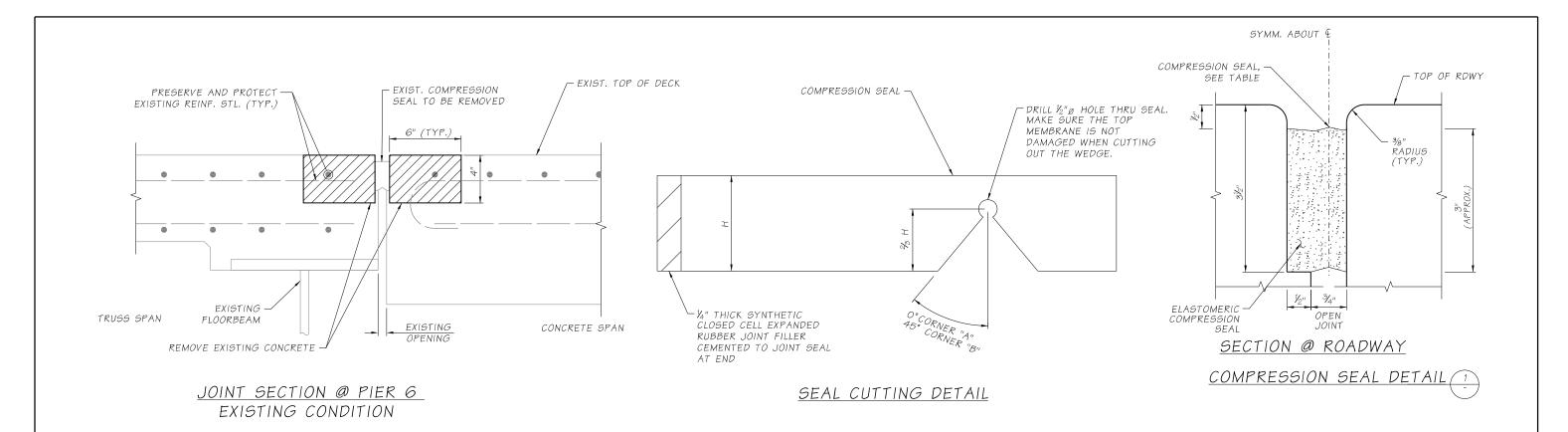
BRIDGE NO. 5/40E

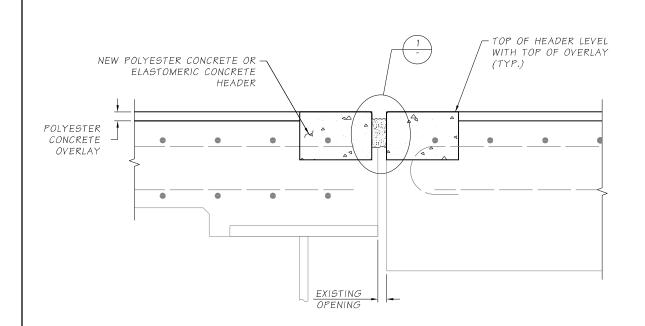
EXPANSION JOINT MODIFICATION AT PIER 3 (FINGER JOINT)

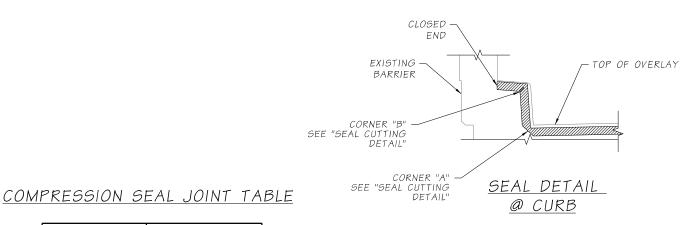
RHC ENGINEERING

84

BA9







D.S. BRO	OWN		N BOWMAN CME
SEAL	WIDTH	SEAL	WIDTH
CV-2250	21/4"	WA-225	21/4"

NOTES:

- 1. ALL EXISTING STEEL REINFORCEMENT AND STRUCTURAL STEEL SHALL BE PRESERVED. NOTIFY ENGINEER FOR ANY OBSERVED DEFECTS.
- 2. TESTING SHALL BE PER ASTM D2628 PRIOR TO USE OF ANY COMPRESSION SEAL.

Ŝ.	FILE NAME X	:\211129 WSDOT SW Bridge Deck &	k Exp\CAD\Bridge 5-40E\DGN\10 BR5-40E BA10 Expansion Joi	nsion Joint Modification Detail at Pier 6.dgn					
<u>~</u> Ш	Bridge Design Engr	A.LELAND				REGION NO.	STATE	FED.AID PROJ.NO.	i
7	Supervisor						WASH	NHFP-0051(316)	ı
<u> </u>	Designed By	G.CHEN				10	WASH	141117-0031(310)	i
	Checked By	J.LI				JOB NUMBER 22X333 CONTRACT NO.			i
	Detailed By	T.LOZADA							i
\ \	Bridge Projects Engr	M.ROSA						LOCATION NO.	i
ñ	Prelim. Plan By							XL6379, XL6380	i
	Architect/Specialist		REVISION	DATE	BY			,	1

JOINT SECTION @ PIER 6

MODIFIED CONDITION

BRIDGE AND **STRUCTURES** OFFICE



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Washington State tment of Transportation

NB AND SB LEWIS RVR BR DECK REPAIR / OVERLAY

I-5

BRIDGE NO. 5/40E

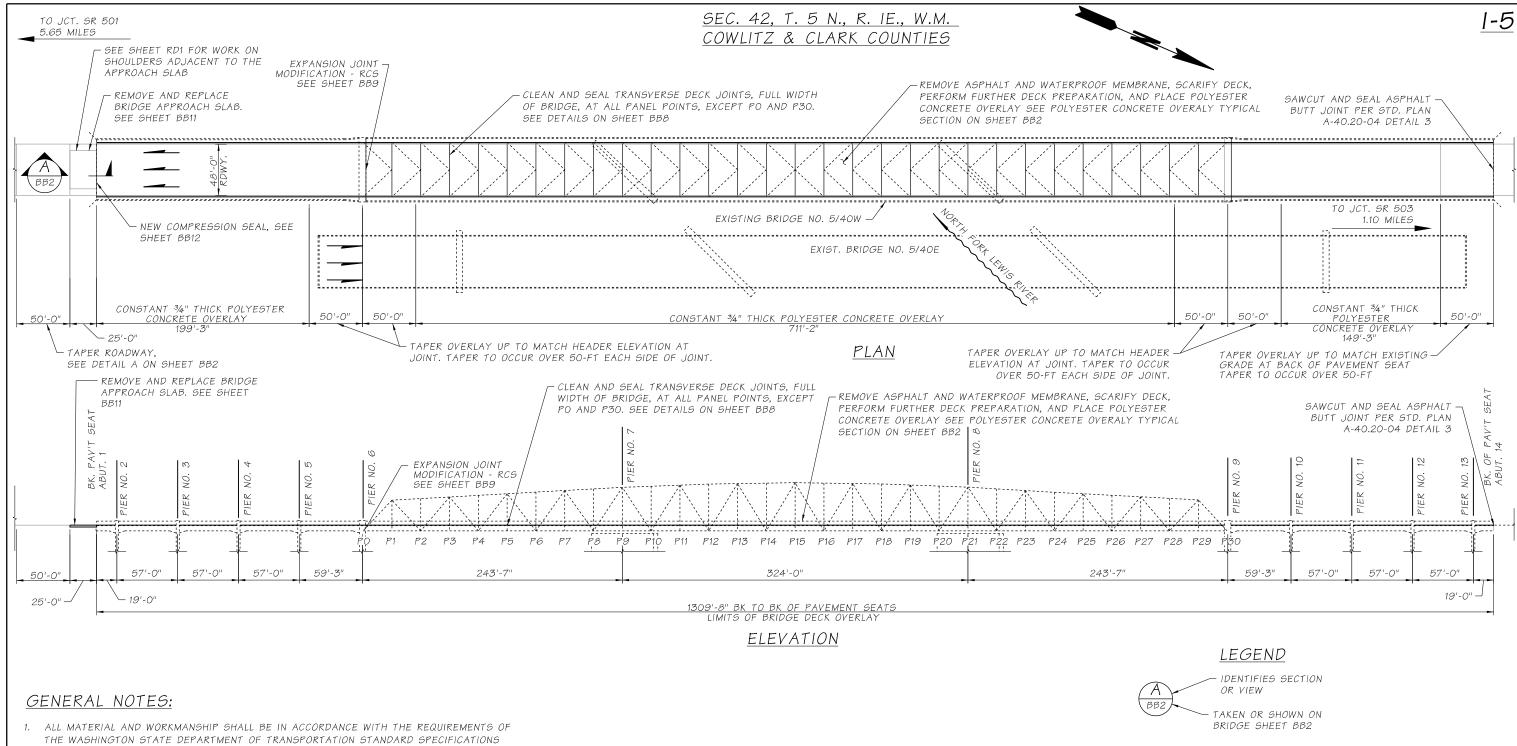
EXPANSION JOINT MODIFICATION AT PIER 6 (COMPRESSION SEAL)

4/14/2023 9:50:38 AM BA10 57

84

BRIDGE SHEET NO.

RHC ENGINEERING



- FOR ROAD, BRIDGE, AND MUNICIPAL CONSTRUCTION, 2023.
- 2. THE REHABILITATION DESIGN HAS BEEN IN ACCORDANCE WITH WSDOT BRIDGE DESIGN MANUAL (2022) AND AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS - 9TH EDITION, 2020.
- 3. EXISTING FEATURES AND DIMENSIONS ARE BASED ON INSPECTION REPORTS AND AS-BUILT PLANS. ALL RELEVANT DIMENSIONS AND EXISTING CONDITIONS SHALL BE FIELD VERIFIED BY THE CONTRACTOR PRIOR TO ORDERING MATERIALS OR STARTING WORK.
- 4. SEE CONSTRUCTION LOAD RESTRICTIONS FOR PAVEMENT REMOVAL ON SHEET BB3.

IDENTIFIES DETAIL TAKEN OR SHOWN ON THE SAME SHEET

> ASPHALT REMOVAL, POLYESTER CONCRETE OVERLAY, JOINT SEALING, AND EXPANSION JOINT MODIFICATION

õ.	FILE NAME X:\211129 WSDOT SW Bridge Deck & Exp\CAD\Bridge 5-40W\DGN\01 BR5-40W BB1 Layout.dgn							
ш	Bridge Design Engr	A.LELAND				REGION NO.	STATE	FED.AID PROJ.NO.
FIL	Supervisor					10	WASH	NHFP-0051(316)
	Designed By	J.LI				10	VVASII	141111 -0031(310)
R	Checked By	G.CHEN				JOB NUMBER 22X333		
	Detalled By	T.LOZADA						
	Bridge Projects Engr	M.ROSA				CONT	RACT NO.	LOCATION NO.
S	Prelim. Plan By							XL6379, XL6380
	Architect/Specialist		REVISION	DATE	BY			

BRIDGE AND **STRUCTURES OFFICE**



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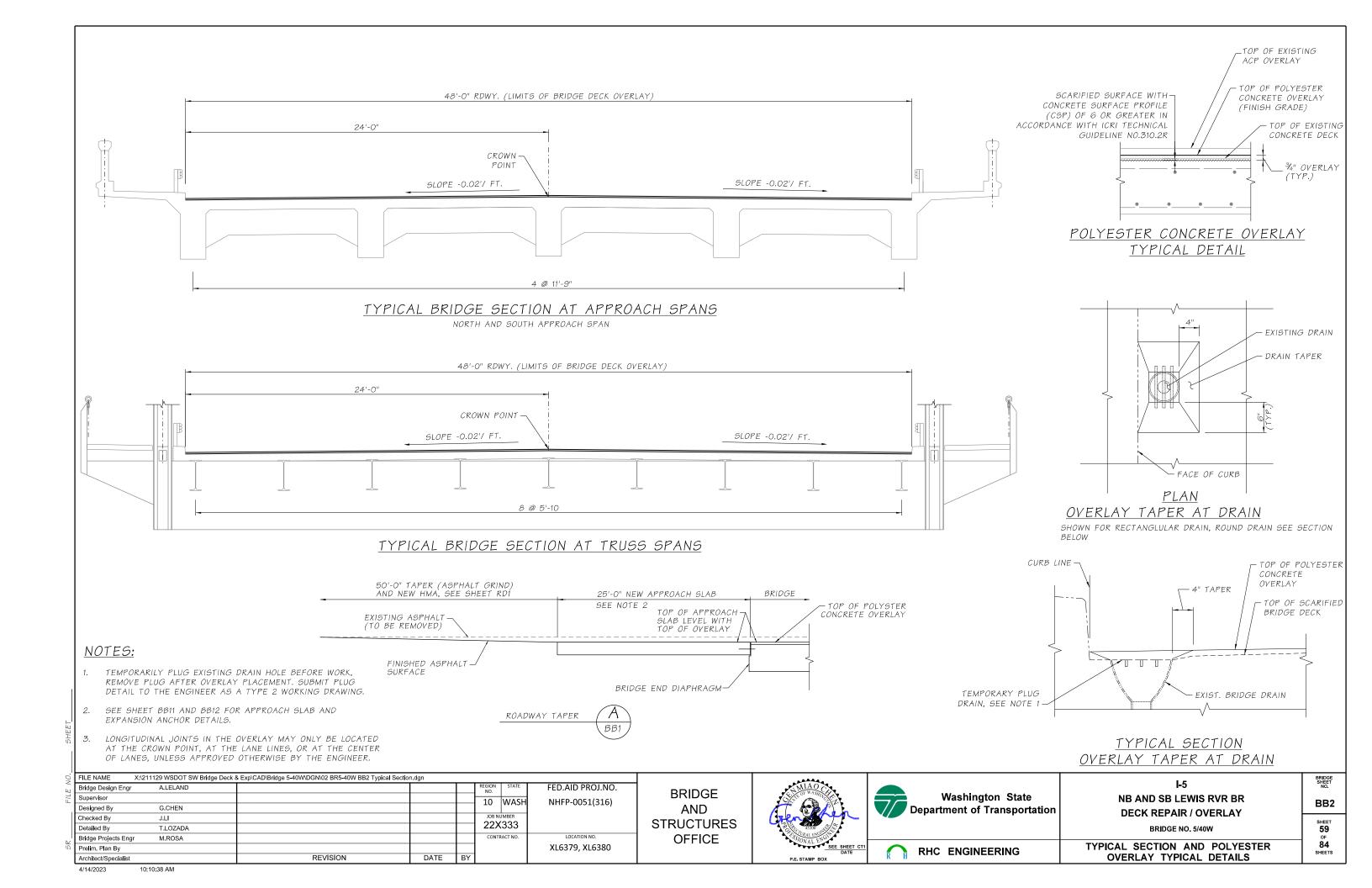
I-5 NB AND SB LEWIS RVR BR DECK REPAIR / OVERLAY

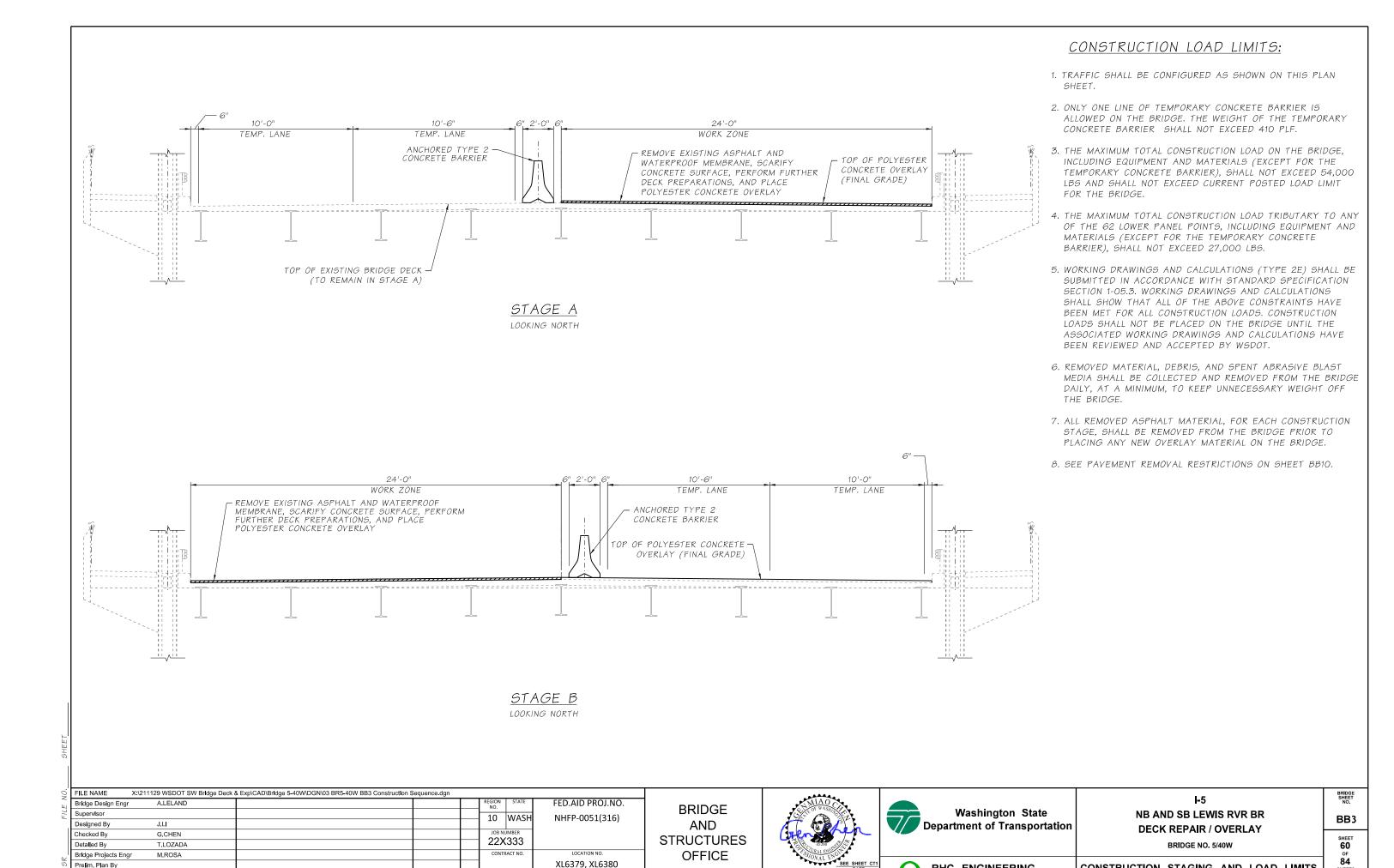
> BRIDGE NO. 5/40W **LAYOUT**

RHC ENGINEERING

58 84

BB1





CONSTRUCTION STAGING AND LOAD LIMITS

RHC ENGINEERING

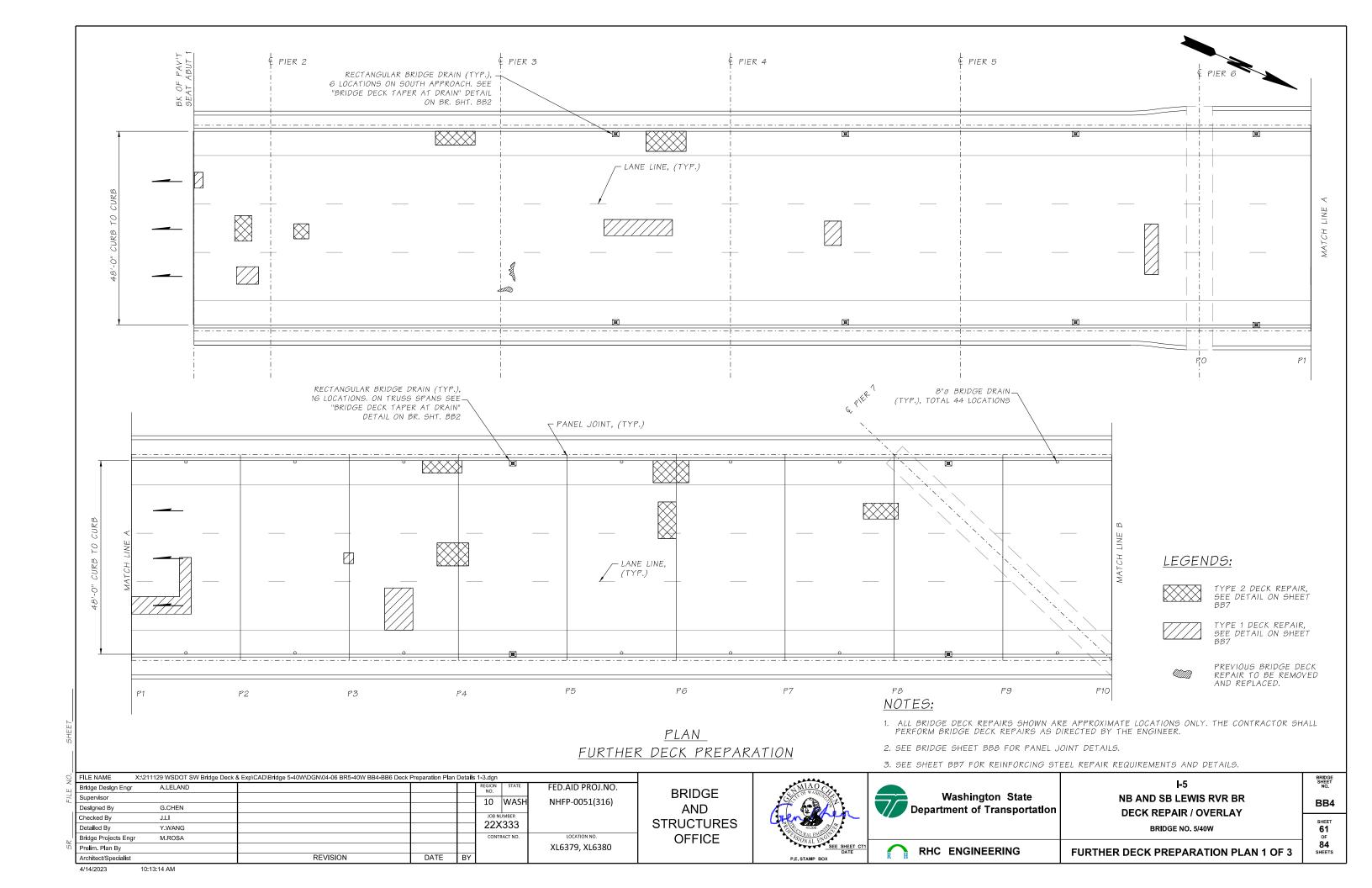
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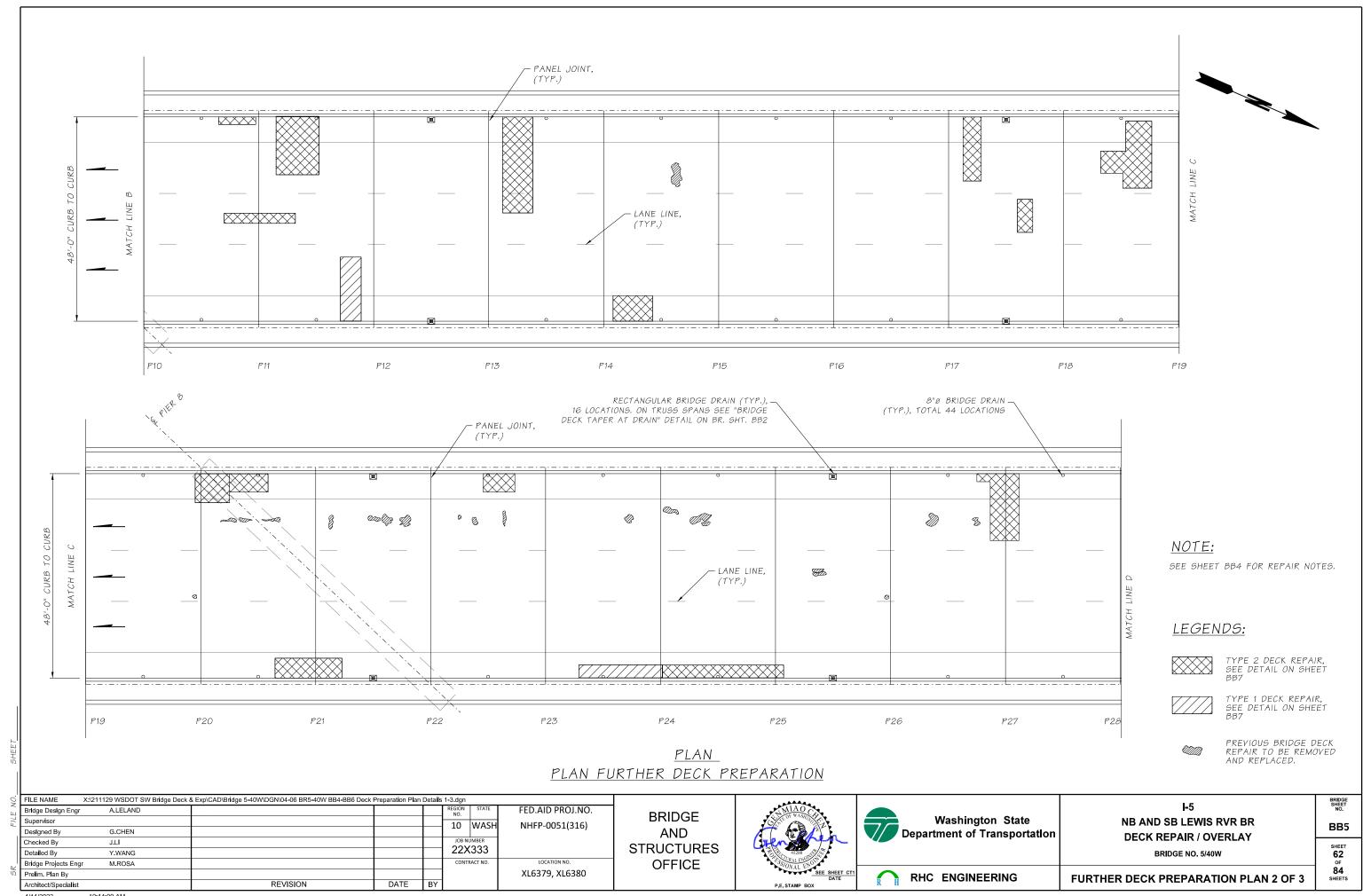
4/14/2023 10:11:29 AM

Architect/Specialist

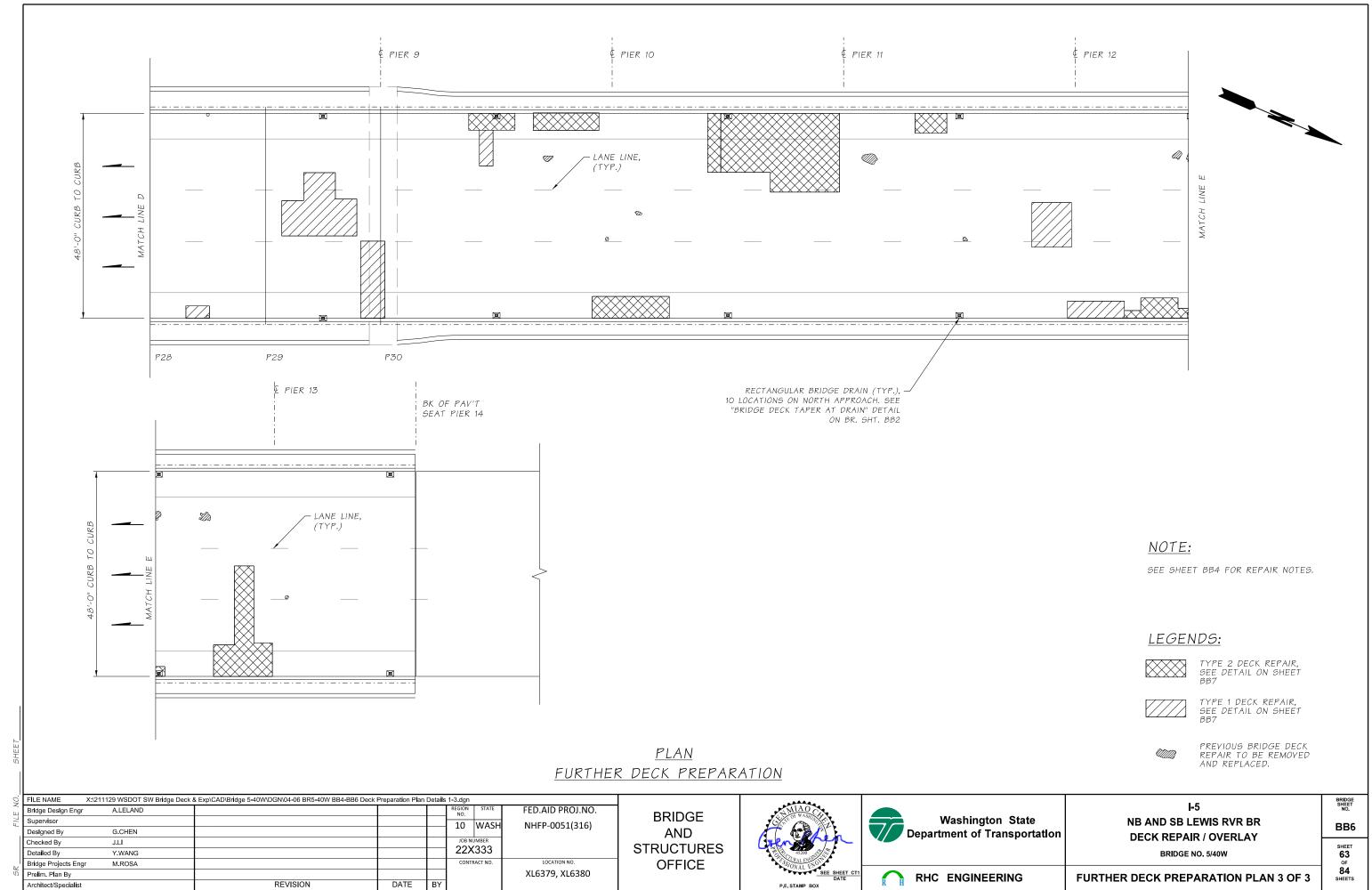
REVISION

DATE BY

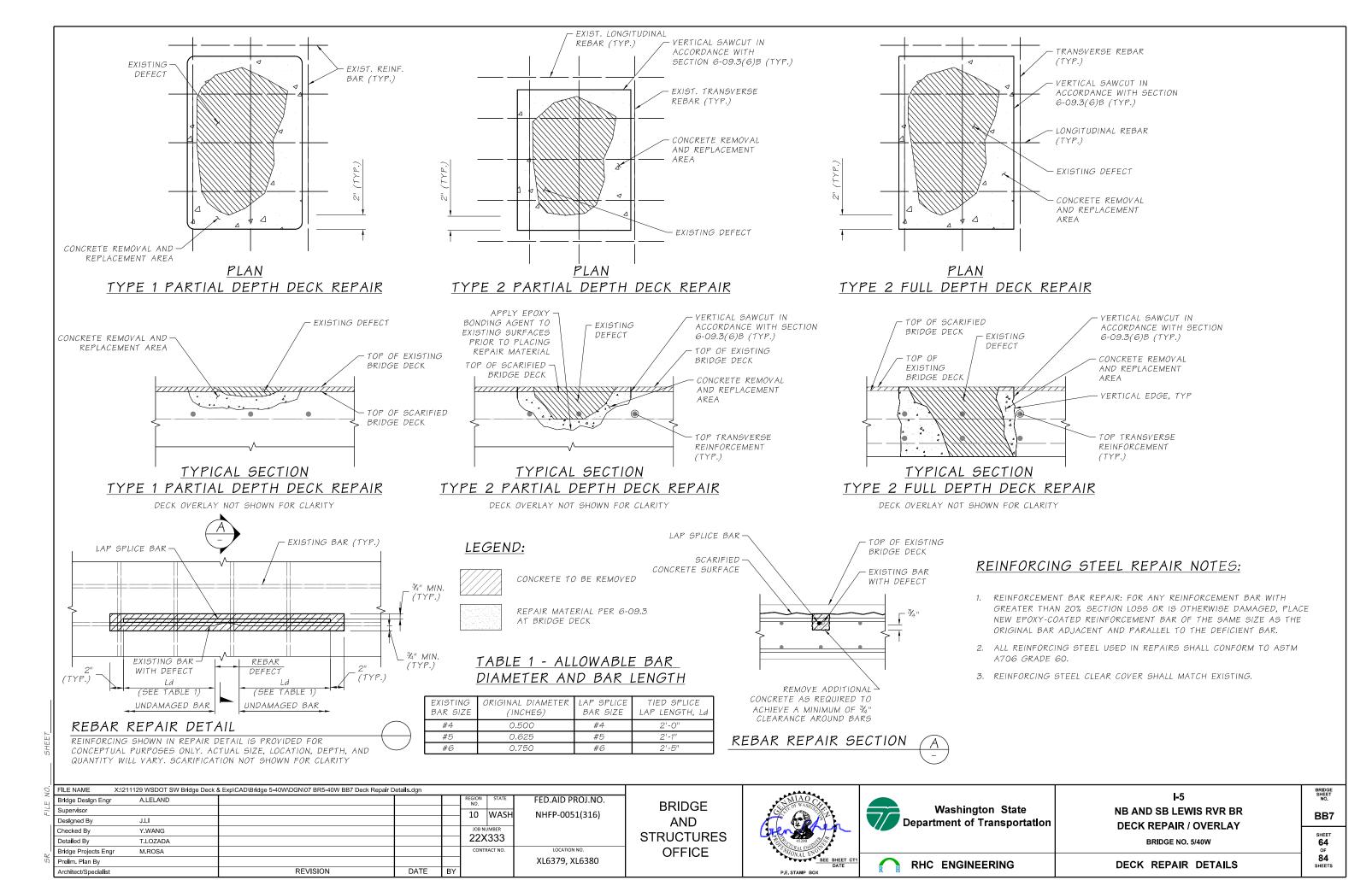




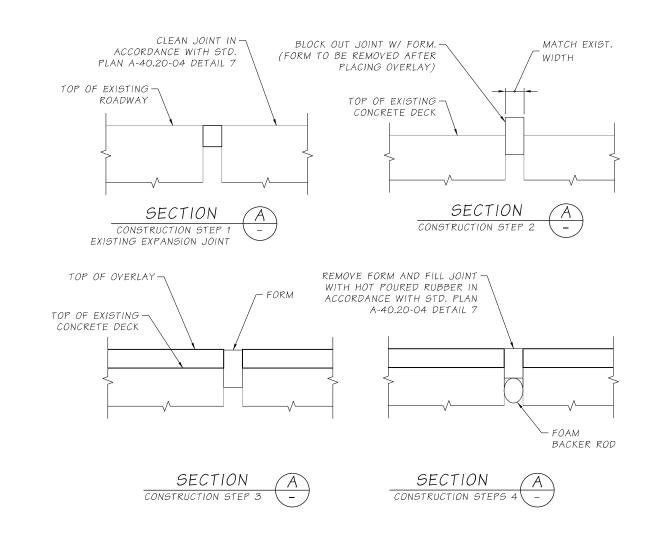
4/14/2023 10:14:00 AM



4/14/2023 10:14:39 AM



4/14/2023 10:15:58 AM



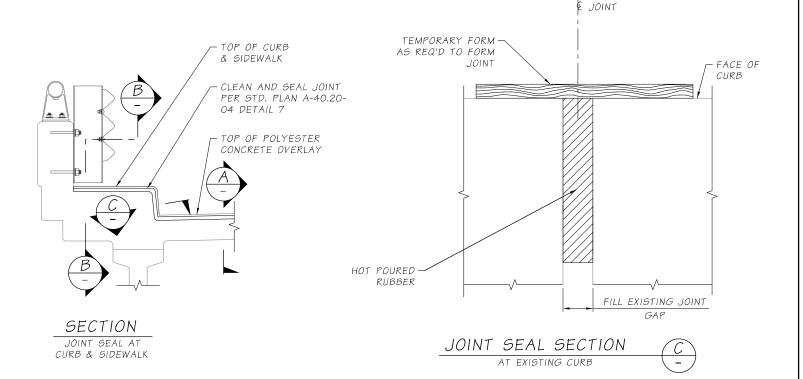
SUGGESTED CONSTRUCTION SEQUENCE:

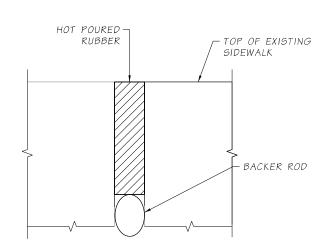
STEP 1. CLEAN JOINT

STEP 2. PLACE FORM IN EXISTING JOINT OPENING.

STEP 3. PLACE POLYESTER CONCRETE OVERLAY TO FINAL RDWY. ELEVATION.

STEP 4. REMOVE FORM FROM JOINT OPENING AND FILL WITH HOT POURED RUBBER.







- 11									
N0.	FILE NAME X:1211129 WSDOT SW Bridge Deck & Exp\CAD\Bridge 5-40W\DGN\08 BR5-40W BB8 Seal Joint Modification Details.dgn								
Z H	Brldge Deslgn Engr	A.LELAND				REGION NO.	STATE	FED.AID PROJ.NO.]
FIL	Supervisor					10	WASH	NHFP-0051(316)	
	Designed By	G.CHEN				10	VVASII	141111 -0051(510)	
	Checked By	J.Ll					UMBER		
	Detailed By	T.LOZADA				22X	(333		·
ע ע	Bridge Projects Engr	M.ROSA				CONTI	RACT NO.	LOCATION NO.	
	Prelim. Plan By							XL6379, XL6380	
	Architect/Specialist		REVISION	DATE	BY				

BRIDGE AND **STRUCTURES** OFFICE



	Washington Sta Department of Transpo
SHEET CT1 DATE	RHC ENGINEERING

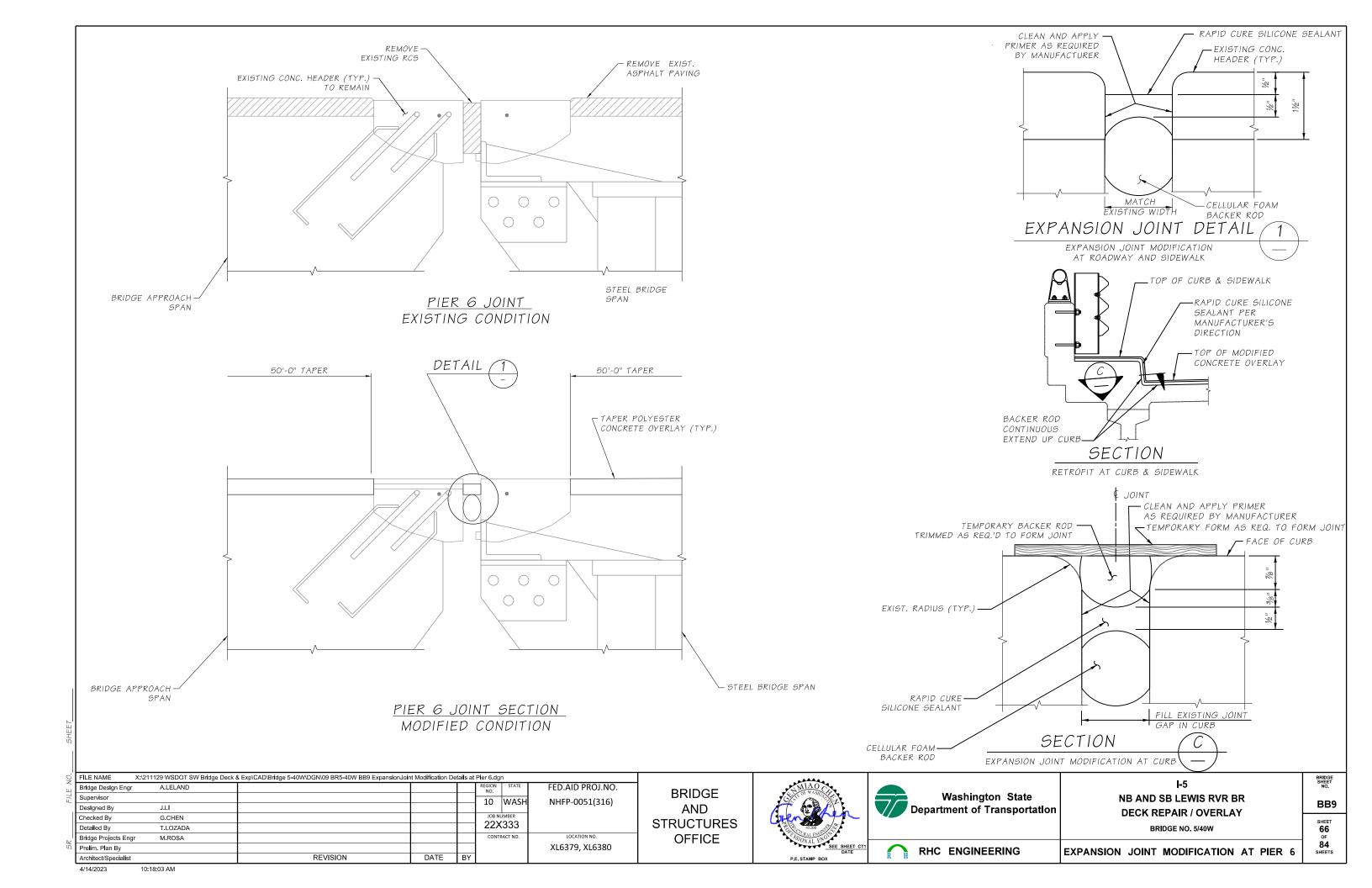
I-5 **NB AND SB LEWIS RVR BR DECK REPAIR / OVERLAY**

BRIDGE NO. 5/40W 84 **DECK JOINT SEALANT DETAIL**

BRIDGE SHEET NO.

BB8

4/14/2023 10:16:49 AM



BRIDGE PAVEMENT REMOVAL NOTES:

- 1. DURING BUTIMUNOUS PAYEMENT REMOVAL WORK ON A BRIDGE, CONSTRUCTION LOADS ON THE BRIDGE SHALL BE RESTRICTED TO THE SPECIFIED BITUMINOUS PAYEMENT REMOVAL TRAIN ONLY ONE REMOVAL TRAIN IS ALLOWED ON A SPAN AT A TIME.
- 2. ALL SPECIFIED MAXIMUM GROSS WEIGHTS ARE THE EQUIPMENTS FULLY LOADED WEIGHT.



BRIDGE NO.	SCRAPER	HAUL TRUCK
	MAX. GROSS WT. (LBS)	MAX. GROSS WT. (LBS)
5/40W	11,000	20,000

PAVEMENT REMOVAL TRAIN - SCRAPER

NO.	FILE NAME >	K:\211129 WSDOT SW Bridge Deck	& Exp\CAD\Bridge 5-40W\DGN\10 BR5-40W BB10 Allowable Pa	aving Train Config	guration	.dgn			
E /	Brldge Deslgn Engr	A.LELAND				REGION NO.	STATE	FED.AID PROJ.NO.	ı
FIL	Supervisor					10	WASH	NHFP-0051(316)	ı
	Designed By	G.CHEN				10	WASII	141111-0031(310)	ı
	Checked By	J.Ll					UMBER		ı
	Detailed By	T.LOZADA				22X	(333		ı
ΩZ I	Bridge Projects Engr	M.ROSA				CONT	RACT NO.	LOCATION NO.	ı
	Prelim. Plan By							XL6379, XL6380	ı
	Architect/Specialist		REVISION	DATE	BY				

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I-5 NB AND SB LEWIS RVR BR DECK REPAIR / OVERLAY

BRIDGE NO. 5/40W

BRIDGE CONSTRUCTION LOAD RESTRICTIONS
HMA OVERLAY REMOVAL

FILE NO. SH

R

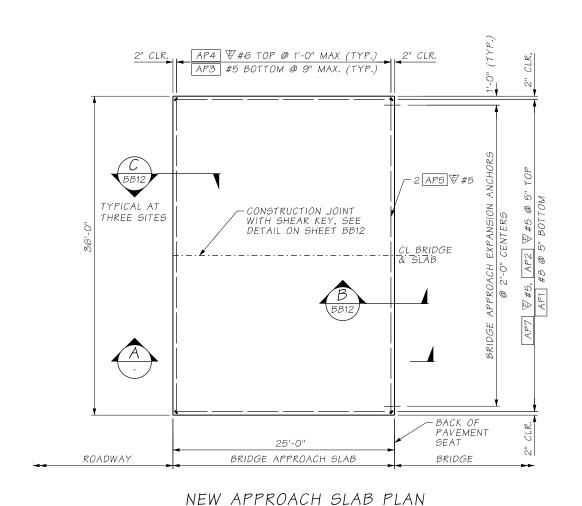
RHC ENGINEERING

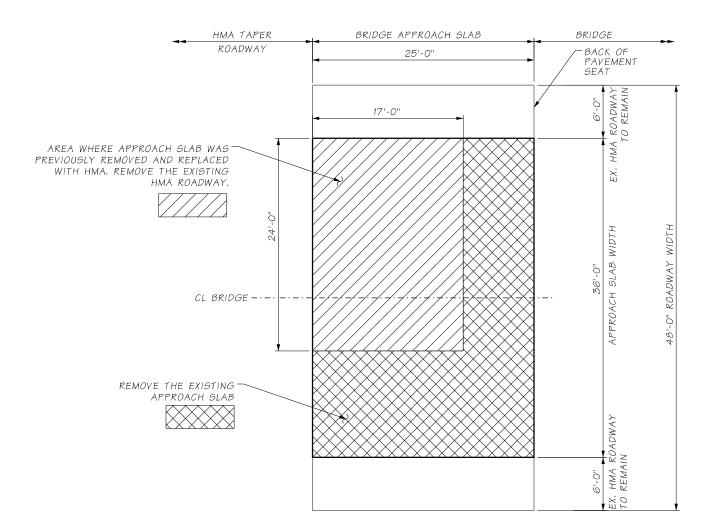
RICTIONS 84 SHEETS

BRIDGE SHEET NO.

BB10

^{SHEET}

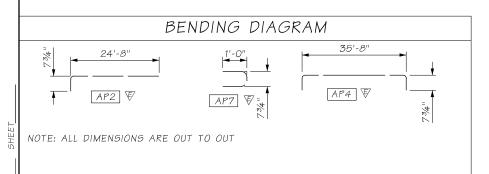


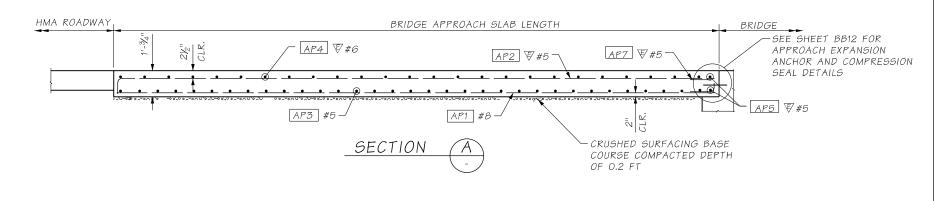


EXISTING APPROACH REMOVAL PLAN

NOTES:

- 1. ALL EDGES OF BRIDGE APPROACH SLAB SHALL HAVE 1/2" RADIUS.
- 2. THE MINIMUM LAP SPLICE OF #5 IS 2'-0", ▼#5 IS 2'-6", ▼#6 IS 3'-0", AND #8 IS 3'-3". ALL LAP SPLICES SHALL BE STAGGERED SO THAT NO MORE THAN 50% OF REBAR IS SPLICED AT THE SAME LOCATION. LAP SPLICES SHALL BE LOCATED WITHIN THE MIDDLE HALF OF THE BRIDGE APPROACH SLAB. OPTIONAL SPLICES ARE ALLOWED FOR AP4 ₹ #6.
- 3. SEE SHEET RD1 FOR WORK ON SHOULDERS THAT ARE ADJACENT TO THE APPROACH SLAB





Ш											
Š	FILE NAME X:\21	1129 WSDOT SW Bridge Deck &	k Exp\CAD\Bridge 5-40W\DGN\11 BR5-40W BB11 Bridge Appro	o\CAD\Bridge 5-40W\DGN\11 BR5-40W BB11 Bridge Approach Slab Details-1.dgn							
_	Bridge Design Engr	A.LELAND				REGION NO.	STATE	FED.AID PROJ.NO.			
1	Supervisor					10	WASH	NHFP-0051(316)			
٠.	Designed By	G.CHEN				10	WASH	NIII F -0031(310)			
	Checked By	J.Ll					JMBER (O.O.O.				
	Detailed By	Y.WANG				22X	(333				
ار	Bridge Projects Engr	M.ROSA				CONTI	RACT NO.	LOCATION NO.			
ñ	Prelim. Plan By							XL6379, XL6380			
	Architect/Specialist		REVISION	DATE	BY						

BRIDGE AND **STRUCTURES OFFICE**



Washington State Department of Transportation

I-5 **NB AND SB LEWIS RVR BR DECK REPAIR / OVERLAY** BRIDGE NO. 5/40W

RHC ENGINEERING

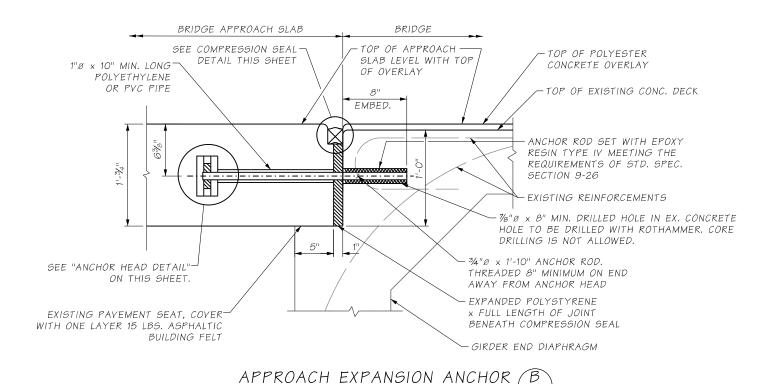
SOUTH END APPROACH SLAB PLAN

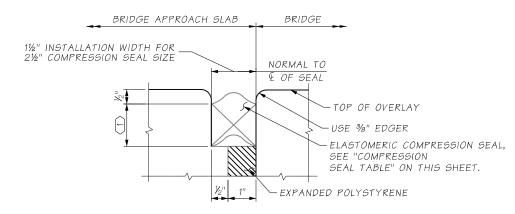
10:19:53 AM 4/14/2023

68 84

BRIDGE SHEET NO.

BB11





COMPRESSION SEAL DETAIL

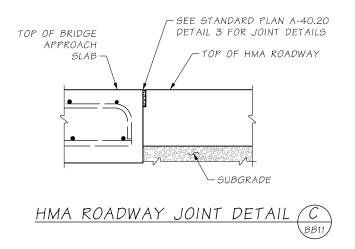
EXPANSION JOINT AT BACK OF PAVEMENT SEAT

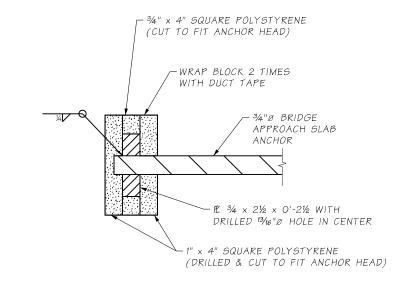
COMPRESSION SEAL TABLE

D.S. BF	ROWN		BOWMAN CME
SEAL	WIDTH	SEAL	WIDTH
CV-2502	21/2	WA-250	21/2

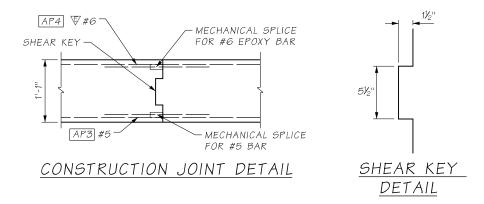
TESTING SHALL BE PER ASTM D 2628 PRIOR TO USE.

T.) FULLY COMPRESSED SEAL HEIGHT, SEAL HEIGHT VARIES WITH MANUFACTUER, VERIFY PRIOR TO SLAB CONSTRUCTION





ANCHOR HEAD DETAIL



NOTES:

- 1. ALL METAL PARTS OF THE APPROACH EXPANSION ANCHOR SHALL RECEIVE ONE COAT OF PAINT CONFORMING TO STANDARD SPECIFICATION SECTION 9-08.1(2)F OR BE GALVANIZED IN ACCORDANCE WITH AASHTO M 232.
- 2. BRIDGE APPROACH SLAB ANCHORS SHALL BE INSTALLED PARALLEL TO ROADWAY AND TO EACH OTHER.
- 3. MECHANICAL SPLICE CAN BE DAYTON D200 SERIES LOCK COUPLER OR D300 SERIES STANDARD COUPLER, OR OTHER EQUIVALENT, SUBMIT SHOP DRAWINGS AND MANUFACTURE PRODUCT CERIFICATE OF COMPLIANCE FOR ENGINEER'S APPROVAL. THE COUPLER FOR TOP BAR SHALL HAVE EPOXY FINISH.

Õ.	FILE NAME X:	\211129 WSDOT SW Bridge D	ck & Exp\CAD\Bridge 5-40W\DGN\12 BR5-40W BB12 Bridge Appro	ach Slab Detalls	-2.dgn			
Ш	Bridge Design Engr	A.LELAND				REGION NO.	STATE	FED.AID PROJ.NO.
H	Supervisor					10	WASH	NHFP-0051(316)
	Designed By	G.CHEN				10	I VVASIII	141111 0031(310)
	Checked By	J.LI				JOB NI		
	Detalled By	Y.WANG				22X	333	
Q∠	Bridge Projects Engr	M.ROSA				CONTRACT NO.		LOCATION NO.
S	Prelim. Plan By						XL6379, XL6380	
	Architect/Specialist		REVISION	DATE	BY			

BRIDGE
AND
STRUCTURES
OFFICE





I-5 NB AND SB LEWIS RVR BR DECK REPAIR / OVERLAY

BRIDGE NO. 5/40W

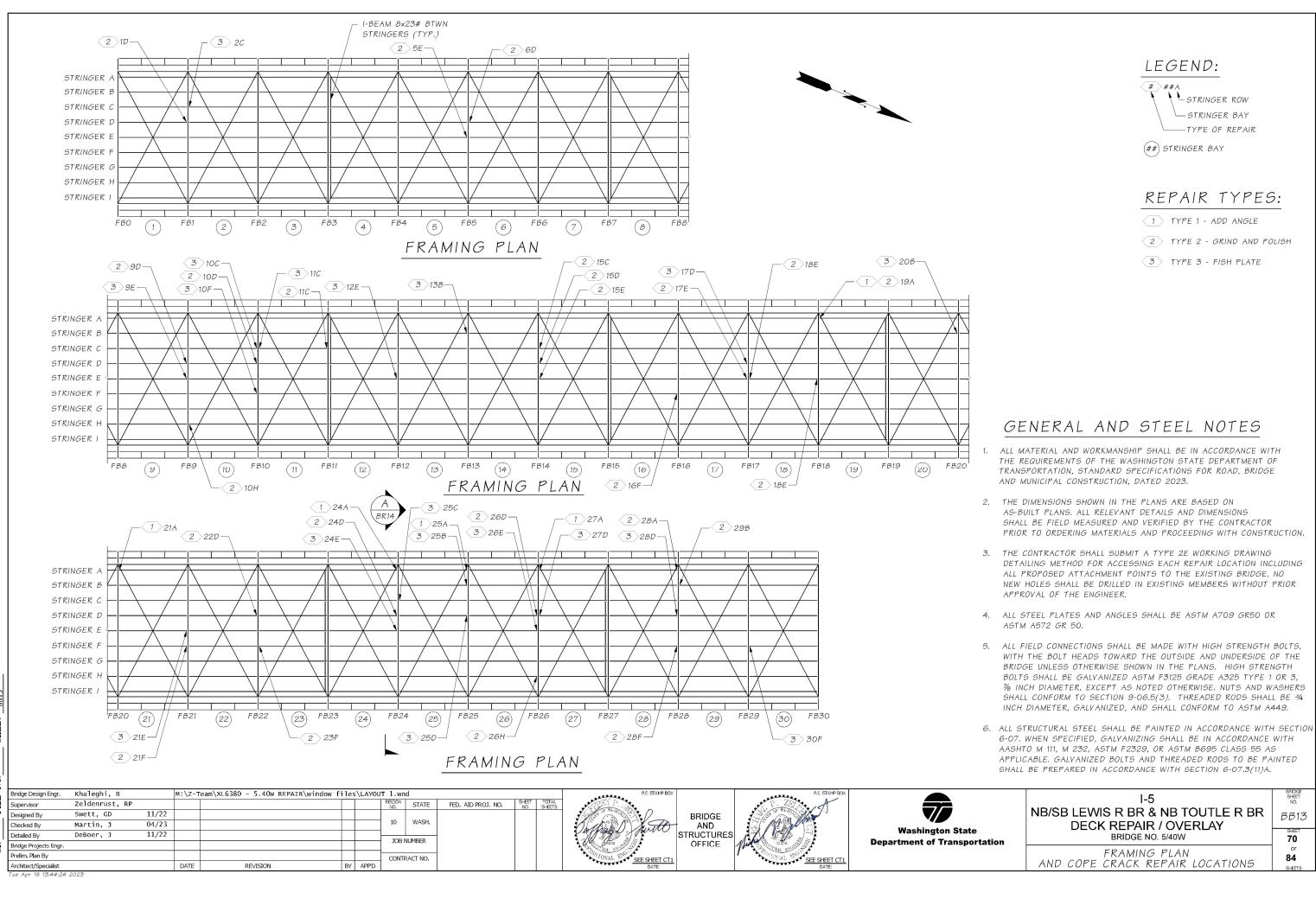
SOUTH END APPROACH SLAB

4/14/2023 10:20:42 AM

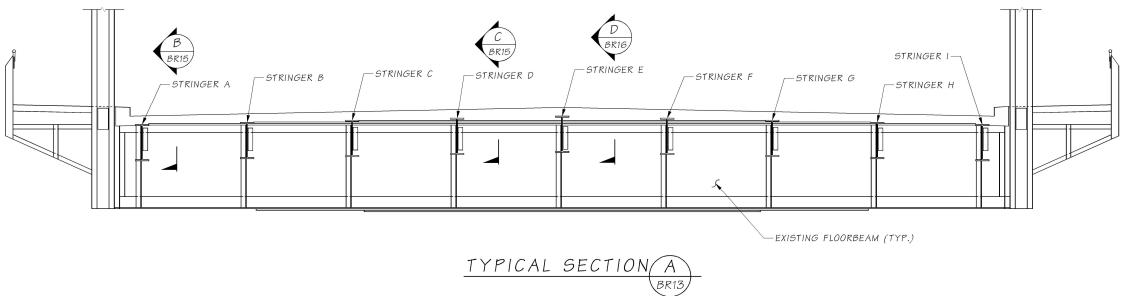
BRIDGE SHEET NO.

RHC ENGINEERING

69 of 84 sheets



SR



	Bridge Design Engr.	Khaleghi, B		M:\Z-T	Z-Team\XL6380 - 5.40w REPAIR\window files\TYP SECTION.wnd									
	Supervisor	Zeldenrust, RP							REGION NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS	
	Designed By	Swett, GD	11/22											
l	Checked By	Martin, J	04/23						10	WASH.				
	Detailed By	DeBoer, J	11/22						TOP	NUMBER				
	Bridge Projects Engr.								JOBI	NOMBER				
	Prelim. Plan By								CONTR	RACT NO.				
	Architect/Specialist			DATE	REVISION		BY	APP'D						

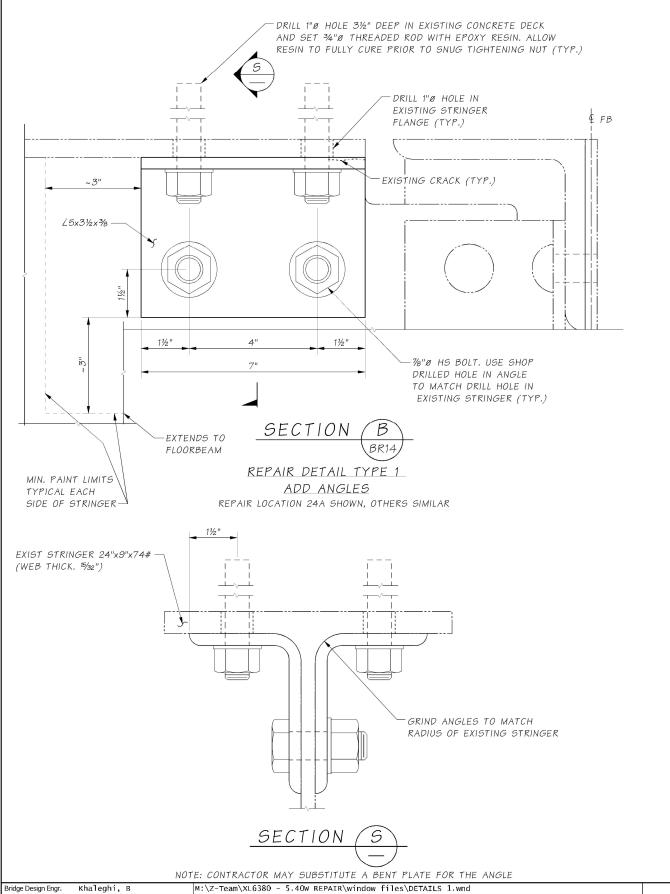
BRIDGE AND STRUCTURES OFFICE



I-5								
NB/SB LEWIS R BR & NB TOUTLE R BR								
DECK REPAIR / OVERLAY								
BRIDGE NO. 5/40W								
7)/710/11 07/07/01/								

BB14 71 of 84

TYPICAL SECTION



STATE

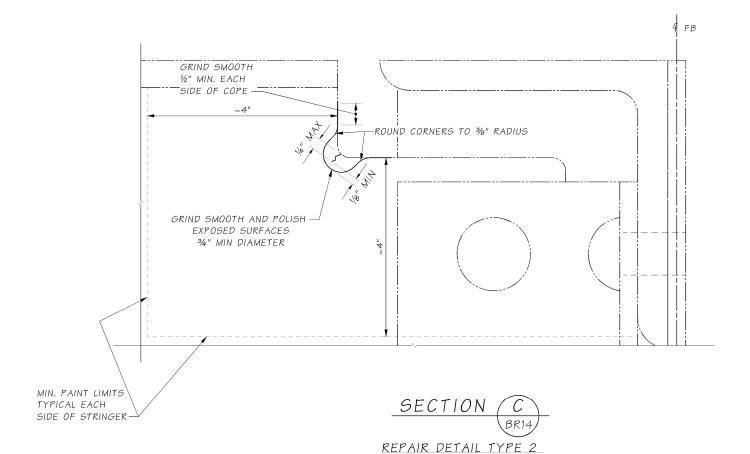
WASH.

JOB NUMBER

CONTRACT NO.

BY APP'D

FED. AID PROJ. NO.



- REMOVE EXISTING PAINT TO BARE METAL. PERFORM MAGNETIC PARTICLE EXAMINATION TO DETERMINE LOCATION AND END OF CRACK (CRACK TIP). IF CRACK LENGTH IS LONGER THAN 14", NOTIFY THE ENGINEER.

GRIND AND POLISH

REPAIR LOCATION 24D SHOWN, OTHERS SIMILAR

- GRING OUT CRACK TO 34" MINIMUM DIAMETER PER DETAIL SHOWN (GRIND MIN. 1/8" BEYOND IDENTIFIED CRACK TIP)
- GRIND SMOOTH ALL SURFACES SHOWN TO 32 RMS FINISH
- REPAIR PAINT



NB/SB LEWIS R BR & NB TOUTLE R BR **DECK REPAIR / OVERLAY** BRIDGE NO. 5/40W

DETAILS

72 84 1 OF 2

BB15

Bridge Projects Engr.

Designed By

Checked By

Detailed By

Prelim. Plan By

Architect/Specialist

zeldenrust, RP

Swett, GD

Martin, J

DeBoer, J

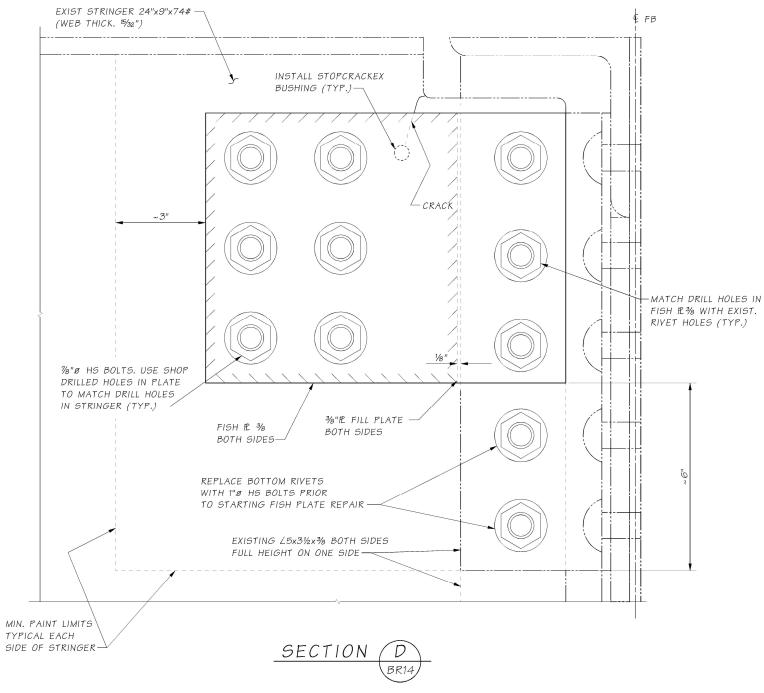
11/22

04/23

11/22

DATE

REVISION



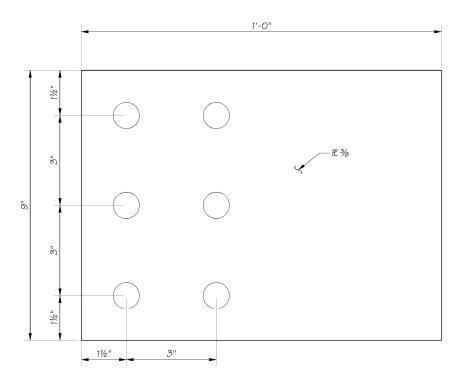
REPAIR DETAIL TYPE 3 FISH PLATE

REPAIR LOCATION 24E SHOWN, OTHERS SIMILAR.

REPAIR NOTES

INSTALL FISHPLATE

- REMOVE EXISTING PAINT AND PERFORM MAGNETIC PARTICLE INSPECTION TO LOCATE END OF CRACK (CRACK TIP).
- INSTALL STOPCRACKEX BUSHING AT 1/16" FROM END OF CRACK PER MANUFACTURER'S RECOMMENDATIONS.
- SURFACE PREPARE AND PRIME PAINT ALL BARE STEEL SURFACES TO BE COVERED BY FISH PLATE.
- RESTRICT TRAFFIC FROM LANE IMMEDIATELY ABOVE STRINGER TO BE REPAIRED.
- REPLACE BOTTOM TWO RIVETS IN SUPPORT ANGLE WITH 1"# HS BOLTS. REAM EXIST.
- BOLT HOLE TO 11/16"Ø.
- REMOVE TOP THREE RIVETS.
- USE SHOP DRILLED HOLES IN FISH PLATE TO MATCH DRILL HOLES IN EXISTING STRINGER AND EXISTING RIVET HOLES TO MATCH DRILL HOLES IN FISH PLATE. BOLT HOLES SHALL BE 15/16"Ø.
- INSTALL FISH PLATES, FILL PLATES AND HIGH STRENGTH BOLTS
- PAINT ALL STEEL SURFACES TO THE LIMITS SHOWN IN THE DETAIL.



FISH PLATE DETAIL

8														
6-2	Bridge Design Engr.	Khaleghi, B	M:\Z-	Team\XL6380 - 5.40w REPAIR\window	files\DETAIL	s 2.wnd			P.E. STAMP BO	×	P.E. STAMP BOX		1.5	BRIDGE SHEET
\equiv	Supervisor	Zeldenrust, RP				REGION STA	E FED. AID PROJ. NO.	SHEET TOTAL NO. SHEETS	JEREY D.		P. ZETA	5	1-J	NO.
E-1.	Designed By	Swett, GD	11/22							BRIDGE	1 2 6 0 MANUTON NO.		NB/SB LEWIS R BR & NB TOUTLE R BR	BB16
	Checked By	Martin, J	04/23			10 WAS	4.		Carlo X TO	AND			DECK REPAIR / OVERLAY	
	Detailed By	DeBoer, J	11/22			TOD NUMBER			Ose year of the	STRUCTURES		Washington State	BRIDGE NO. 5/40W	73
뇄	Bridge Projects Engr.					JOB NUMBER			12820 3/3	OFFICE	1	Department of Transportation		- 75 OF
U)	Prelim. Plan By					CONTRACT NO).		SEE SHEET CT	,	YONAL THE SEE SHEET CT1		DETAILS	84
	Architect/Specialist		DATE	REVISION	BY APP'D				DATE:	-	DATE:		2 OF 2	SHEETS

Tue Apr 18 13:44:25 2023



STRINGER 21A AT FB20



STRINGER 24A AT FB24



STRINGER 25A AT FB25

 $\langle 1 \rangle$



STRINGER 27A AT FB26



STRINGER 19A AT FB18

REPAIR TYPES:

- 1 TYPE 1 ADD ANGLE
- 2 TYPE 2 GRIND AND POLISH
- 3 TYPE 3 FISH PLATE

a	Bridge Design Engr.	Khaleghi, B		M:\Z-T	eam\XL6380 - 5.40w REPAIR\window	files	\PICS	1.wnd					
3	Supervisor	Zeldenrust, RP						REGION NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS	
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	Detailed By	DeBoer, J	12/22					TOD A	ILIMDED				
4	Bridge Projects Engr.							JOB NUMBER					
2	Prelim. Plan By							CONTR	RACT NO.				
	Architect/Specialist			DATE	REVISION	BY	ΔPP'D					1	



BRIDGE AND STRUCTURES OFFICE





I-5
NB/SB LEWIS R BR & NB TOUTLE R BR
DECK REPAIR / OVERLAY
BRIDGE NO. 5/40W

BRIDGE
SHEET
74

SITE PHOTOS 1 OF 8 74 or 84 sheets

SR FILE N

Tue Apr 18 13:44:25 2023



STRINGER 1D AT FB1 $\langle 2 \rangle$



STRINGER 5E AT FB5 2



STRINGER 6D AT FB5 2



STRINGER 9D AT FB9 (2)



STRINGER 10H AT FB9 (2)



STRINGER 10D AT FB10 (2)

- 1 TYPE 1 ADD ANGLE
- 2 TYPE 2 GRIND AND POLISH

3 TYPE 3 - FISH PLATE

a	Bridge Design Engr.	Khaleghi, B		M:\Z-T	eam\XL6380 - 5.40w REPAIR\window	file	s\P I CS	2.wnd					Τ
3	Supervisor	Zeldenrust, RP						REGION NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS	
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	Architect/Specialist			DATE	DE//ISION	BY	APP'D					1	

BRIDGE AND STRUCTURES OFFICE





I-5 NB/SB LEWIS R BR & NB TOUTLE R BR DECK REPAIR / OVERLAY BRIDGE NO. 5/40W

SITE PHOTOS 2 OF 8

75 or 84

BB18

STRINGER 11C AT FB11 (2)



STRINGER 15C AT FB14 (2)



STRINGER 15D AT FB14 (2)



STRINGER 15E AT FB14 (2)



STRINGER 16F AT FB16 $\langle 2 \rangle$



STRINGER 17E AT FB17 (2)

- 1 TYPE 1 ADD ANGLE
- 2 TYPE 2 GRIND AND POLISH
- 3 TYPE 3 FISH PLATE

a	Bridge Design Engr.	Khaleghi, B		M:\Z-T	eam\XL6380 - 5.40w REPAIR\window	files	\PICS	3.wnd					Τ
3	Supervisor	Zeldenrust, RP						REGION NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS	
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	Checked By	Martin, J	04/23					10	WASH.				
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	Architect/Specialist			DATE	REVISION .	BY	APP'D					1	

BRIDGE AND STRUCTURES OFFICE





I-5 NB/SB LEWIS R BR & NB TOUTLE R BR DECK REPAIR / OVERLAY BRIDGE NO. 5/40W

SITE PHOTOS 3 OF 8

BB19

76 or 84

STRINGER 18E AT FB17



STRINGER 18E AT FB18



STRINGER 22D AT FB22



STRINGER 23F AT FB22



STRINGER 24D AT FB24



STRINGER 26D AT FB26

- 1) TYPE 1 ADD ANGLE
- 2 TYPE 2 GRIND AND POLISH
- 3 TYPE 3 FISH PLATE

Bridge Design Engr.	Khaleghi, B		M:\Z-T	eam\XL6380 - 5.40w REPAIR\window	files	\PICS	4.wnd				
Supervisor	Zeldenrust, RP						REGION NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
Designed By	Swett, GD	12/22									
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Date

BRIDGE AND STRUCTURES OFFICE





I-5 NB/SB LEWIS R BR & NB TOUTLE R BR DECK REPAIR / OVERLAY BRIDGE NO. 5/40W

SITE PHOTOS 4 OF 8



STRINGER 26H AT FB26



STRINGER 28A AT FB28



STRINGER 28F AT FB28

(2)



STRINGER 29B AT FB29

REPAIR TYPES:

1 TYPE 1 - ADD ANGLE

2 TYPE 2 - GRIND AND POLISH

3 TYPE 3 - FISH PLATE

4													
,	Bridge Design Engr.	Khaleghi, B		M:\Z-T	Team\XL6380 - 5.40w REPAIR\window	files	\PICS	5.wnd					
1	Supervisor	Zeldenrust, RP						REGION NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS	
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2	Prelim. Plan By							CONTR	RACT NO.				
	Architect/Specialist			DATE	REVISION	BY	APP'D					1	

BRIDGE AND STRUCTURES OFFICE





I-5 NB/SB LEWIS R BR & NB TOUTLE R BR DECK REPAIR / OVERLAY BRIDGE NO. 5/40W

SITE PHOTOS 5 OF 8 BB21 78 or 84

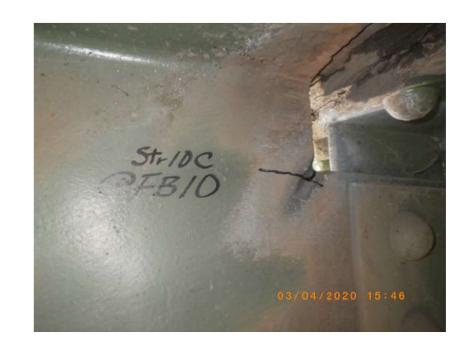
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Tue Apr 18 13:44:58 2023

STRINGER 2C AT FB1



STRINGER 9E AT FB9



STRINGER 10C AT FB10

3



STRINGER 10F AT FB10



STRINGER 11C AT FB10



STRINGER 12E AT FB12

REPAIR TYPES:

- 1 TYPE 1 ADD ANGLE
- 2 TYPE 2 GRIND AND POLISH
- 3 TYPE 3 FISII PLATE

Γ	Bridge Design Engr.	Khaleghi, B		M:\Z-T	eam\XL6380 - 5.40w REPAIR\window	files	S\PICS	6.wnd				
Γ	Supervisor	Zeldenrust, RP						REGION NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
٦,	Designed By	Swett, GD	12/22									
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BRIDGE AND STRUCTURES OFFICE





I-5 NB/SB LEWIS R BR & NB TOUTLE R BR DECK REPAIR / OVERLAY BRIDGE NO. 5/40W

SITE PHOTOS 6 OF 8 BB22 SHEET **79** OF

Tue Apr 18 13:45:11 202

STRINGER 13B AT FB13



STRINGER 17D AT FB17



STRINGER 20B AT FB20

3



STRINGER 21E AT FB21



STRINGER 24E AT FB24



STRINGER 25C AT FB24

3

REPAIR TYPES:

- 1 TYPE 1 ADD ANGLE
- 2 TYPE 2 GRIND AND POLISH
- 3 TYPE 3 FISH PLATE

a	Bridge Design Engr.	Khaleghi, B		M:\Z-T	eam\XL6380 - 5.40w REPAIR\window	files	\PICS	7.wnd					Τ
3	Supervisor	Zeldenrust, RP						REGION NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS	
٩.	Designed By	Swett, GD	12/22										
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Š	Bridge Projects Engr.							JOBI	NOMBEK				
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SEE SHEET CT1

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BRIDGE AND STRUCTURES OFFICE





I-5 NB/SB LEWIS R BR & NB TOUTLE R BR DECK REPAIR / OVERLAY BRIDGE NO. 5/40W

SITE PHOTOS 7 OF 8 BB23 80 or 84

SK FILE

Architect/Specialist
Tue Apr 18 13:45:24 2023

STRINGER 25B AT FB25 3



STRINGER 25D AT FB25 3



STRINGER 26E AT FB26 3



STRINGER 27D AT FB26 3



STRINGER 28D AT FB28 (3)



STRINGER 30F AT FB29 3

- 1 TYPE 1 ADD ANGLE
- 2 TYPE 2 GRIND AND POLISH

BB24

81 of

3 TYPE 3 - FISH PLATE

a	Bridge Design Engr.	Khaleghi, B		M:\Z-T	eam\XL6380 - 5.40w REPAIR\window	files	\PICS	8.wnd					Τ
3	Supervisor	Zeldenrust, RP						REGION NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS	
٩.	Designed By	Swett, GD	12/22										
	Checked By	Martin, J	04/23					10	WASH.				
	Detailed By	DeBoer, J	12/22					TOD A	NUMBER				
4	Bridge Projects Engr.							JOBI	NOMBER				
2	Prelim. Plan By							CONTR	RACT NO.				
	Architect/Specialist			DATE	REVISION .	BY	APP'D						

BRIDGE AND STRUCTURES OFFICE





I-5 NB/SB LEWIS R BR & NB TOUTLE R BR DECK REPAIR / OVERLAY BRIDGE NO. 5/40W

SITE PHOTOS 8 OF 8

	MINI	MUM	LANE	CLOS	JRE T	APER	LENG1	ΓH = L	_ (feet)
LANE				Pos	ted Sp	eed (n	nph)			
WIDTH (feet)	25	30	35	40	45	50	55	60	65	70
10	105	150	205	270	450	500	550	-	-	-
11	115	165	225	295	495	550	605	660	-	-
12	125	180	245	320	540	600	660	720	780	840

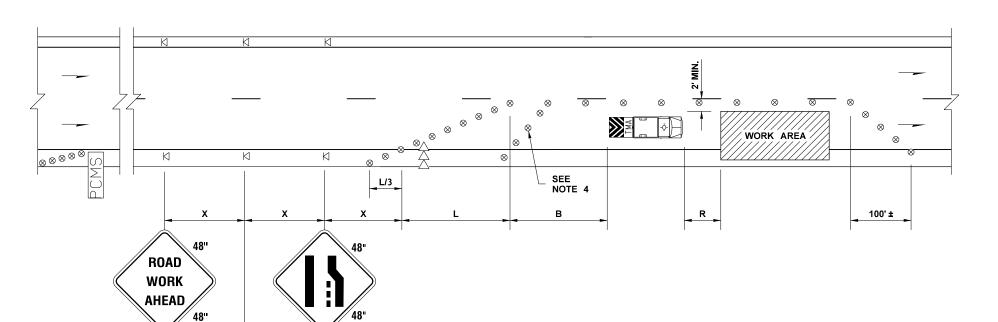
	MIN	NIMUM	SHOU	JLDER	TAPE	R LEN	NGTH	= L/3	(feet)	
SHOULDER				Pos	ted Sp	eed (n	nph)			
WIDTH (feet)	25	30	35	40	45	50	55	60	65	70
8'	40	40	60	90	120	130	150	160	170	190
10'	40	60	90	90	150	170	190	200	220	240
	USE A	MINIMUM	3 DEV	ICES TA	APER FO	OR SHO	ULDER	LESS TH	IEN 8'.	•

SIGN SPACIN	G = X (1)	
FREEWAYS & EXPRESSWAYS	55 / 70 MPH	1500' ±
RURAL HIGHWAYS	60 / 65 MPH	800' ±
RURAL ROADS	45 / 55 MPH	500' ±
RURAL ROADS & URBAN ARTERIALS	35 / 40 MPH	350' ±
RURAL ROADS & URBAN ARTERIALS	25 / 30 MPH	200' ± (2)
RESIDENTIAL & BUSINESS DISTRICTS		
URBAN STREETS	25 MPH OR LESS	100' ± (2)
(1) ALL SPACING MAY BE ADJUSTED TO	ACCOMMODATE IN	TERCHANGE

1)	ALL	SPACING	MAY BE	E ADJUSTED	ТО	ACCON	/MODATE	INT	ERCHANG	Ε
		RAMPS, AT	-GRADE	INTERSECT	IONS	AND I	DRIVEWA'	YS.		
2)	THIS	SPACING	MAY E	BE REDUCE	O IN	URBAN	AREAS	TO	FIT	
ĺ		ROADWAY	CONDI	ΓIONS.						

	LIZATION ACING (fe	
MPH	TAPER	TANGENT
50/70	40	80
35/45	30	60
25/30	20	40

	BUFFER DATA										
LONGITUDINAL BUFFER SPACE = B											
SPEED (MF	PH)	25	30	35	40	45	50	55	60	65	70
LENGTH (fe	et)	155	200	250	305	360	425	495	570	645	730
TRANSPO	ORTAE	BLE A	TTEN	UATO	R RO	LL AI	HEAD	DIST	ANCE	= R	
		LE WEI				ı		VEHICLE 22,000		ЭНТ	
< 45 MPH	45-55	MPH	> !	55 MPH	l <	45 MP	н	45-55 N	1PH	> 55 I	MPH
100' 123' 172' 74' 100' 150'											



PCMS									
1	2								
RIGHT LANE CLOSURE	1 MILE AHEAD								
2.0 SEC	2.0 SEC								

FIELD LOCATE 1 MILE ± IN ADVANCE OF LANE CLOSURE SIGNING.

NOTES

- 1. SEE SPECIAL PROVISIONS FOR WORK HOUR RESTRICTIONS.
- 2. EXTEND DEVICE TAPER AT L/3 ACROSS SHOULDER.
- 3. DEVICES SHALL NOT ENCROACH INTO THE ADJACENT LANE.
- 4. USE TRANSVERSE DEVICES IN CLOSED LANE EVERY 1000'(FT) (RECOMMENDED).
- 5. DEVICE SPACING FOR THE DOWNSTREAM TAPER SHALL BE 20' (FT).
- 6. ALL SIGNS ARE BLACK ON ORANGE.

SINGLE-LANE CLOSURE FOR MULTI-LANE ROADWAYS

PCMS

LEGEND

PORTABLE CHANGEABLE MESSAGE SIGN

TEMPORARY SIGN LOCATION

TRAFFIC SAFETY DRUM

TRANSPORTABLE ATTENUATOR

SEQUENTIAL ARROW SIGN

W20-1

RIGHT LANE

CLOSED AHEAD

W20-5R

NOT TO SCALE

FILE NAME	\\Wsdot.loc\sw\Group\Englnee	rlng\444301\05-DesIgn\02-Projects\I-5 NB & SB LewIs F	River Br and	NB To	outle River Br_I	Deck Repair\1-CADD-Plans_Co	tractPlans\22X333_PS_TCP.dgn			
TIME	10:01:49 AM				REGION STATE	FED.AID PROJ.NO.	1			I-5
DATE	3/14/2023				10 WASH	NHFP-0051(316)				
PLOTTED BY	hillb] IU WASH					NB AND SB LEWIS RVR BR
DESIGNED BY	T. PANKAU				JOB NUMBER				Washington State	DECK REPAIR / OVERLAY
ENTERED BY	CCEO				22X333				_	
CHECKED BY	A. ROMANCHOCK				CONTRACT NO.	LOCATION NO.	1		Department of Transportation	
PROJ. ENGR.	S. FELL				1		DATE	DATE	-	TRAFFIC CONTROL PLAN
REGIONAL ADM.	C. FRANCIS	REVISION	DATE	BY	1		P.E. STAMP BOX	P.E. STAMP BOX		

TC1 82

Plot 1

PLAN REF NO

MINIM	MINIMUM LANE CLOSURE TAPER LENGTH = L (feet)											
LANE												
WIDTH (feet)	25	30	35	40	45	50	55	60	65	70		
10	105	150	205	270	450	500	550	-	-	-		
11	115	165	225	295	495	550	605	660	-	-		
12	125	180	245	320	540	600	660	720	780	840		

	MINIMUM SHOULDER TAPER LENGTH = L/3 (feet)												
SHOULDER Posted Speed (mph)													
(feet)	25	25 30 35 40 45 50 55 60 65 70											
8'	40	40	60	90	120	130	150	160	170	190			
10'	10' 40 60 90 90 150 170 190 200 220 240												
	LISE A MINIMUM 3 DEVICES TAPER FOR SHOULDER LESS THEN 8'												

i = X (1)	
55 / 70 MPH	1500' ±
60 / 65 MPH	800' ±
45 / 55 MPH	500' ±
35 / 40 MPH	350' ±
25 / 30 MPH	200' ± (2)
25 MPH OR LESS	100' ± (2)
	55 / 70 MPH 60 / 65 MPH 45 / 55 MPH 35 / 40 MPH 25 / 30 MPH

(1) ALL SPACING MAY BE ADJUSTED TO ACCOMMODATE INTERCHANGE RAMPS, AT-GRADE INTERSECTIONS AND DRIVEWAYS. (2) THIS SPACING MAY BE REDUCED IN URBAN AREAS TO FIT ROADWAY CONDITIONS.

	BUFFER DATA											
LONGITUDINAL BUFFER SPACE = B												
SPEED (MPH)	25	30	35	40	45	50	55	60	65	70		
LENGTH (feet)	155	200	250	305	360	425	495	570	645	730		
TRANSPORTABLE ATTENUATOR ROLL AHEAD DISTANCE = R												

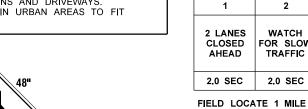
		TRANS	PORTABLE A	ATTENUATOR	R ROLL AHE	AD DISTAN	CE :				
PC	MS		HOST VEHICLE WEIGHT HOST VEHICLE W								
	2	9,90	00 TO 22,000 lb:	S.		> 22,000 lbs.					
NES	WATCH	< 45 MPH	45-55 MPH	> 55 MPH	< 45 MPH	45-55 MPH	>				
SED AD	FOR SLOW TRAFFIC	100'	123'	172'	74'	100'					

NOTES

(RECOMMENDED).

6. ALL SIGNS ARE BLACK ON ORANGE.

20' (FT).



FIELD LOCATE 1 MILE ± IN ADVANCE OF LANE CLOSURE SIGNING.

CHANNELIZATION DEVICE SPACING (feet)										
TAPER	TANGENT									
40	80									
30	60									
20	40									
	PACING (fe TAPER 40 30									

1. SEE SPECIAL PROVISIONS FOR WORK HOUR RESTRICTIONS.

3. DEVICES SHALL NOT ENCROACH INTO THE ADJACENT LANES.

5. DEVICE SPACING FOR THE DOWNSTREAM TAPER SHALL BE

4. USE TRANSVERSE DEVICES IN CLOSED LANE EVERY 1000'(FT)

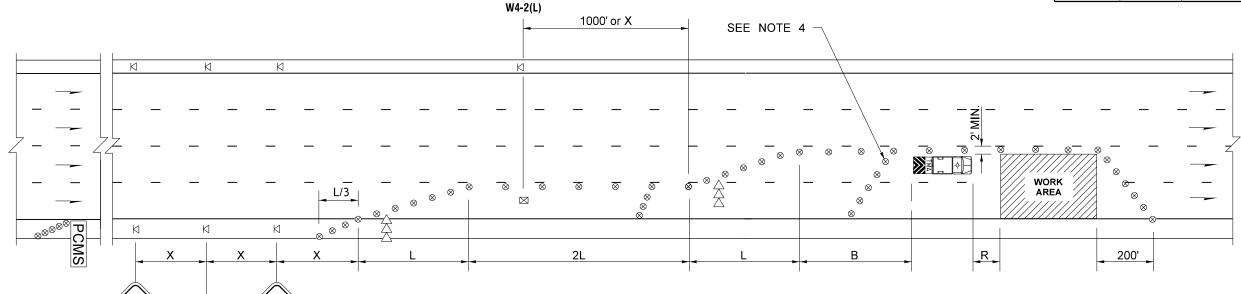
2. EXTEND DEVICE TAPER AT L/3 ACROSS SHOULDER.

55 MPH

Plot 2 PLAN REF NO TC2

> SHEET 83 84 SHEETS

150'



LEGEND

- TRAFFIC SAFETY DRUM
- TEMPORARY SIGN LOCATION
- SEQUENTIAL ARROW SIGN

DOUBLE-LANE CLOSURE FOR MULTI-LANE ROADWAYS

PCMS PORTABLE CHANGEABLE MESSAGE SIGN

TRANSPORTABLE ATTENUATOR

ROAD WORK

AHEAD

W20-1

RIGHT

TWO LANES

CLOSED AHEAD

W20-501

W4-2(L)

	TEMPORARY SIGN L	OCATION (5'(FT) MOUNTING HEIGHT)					NOT TO SCALE				
FILE NAME	\\Wsdot.loc\sw\Group\Englne	ering\444301\05-Design\02-Projects\I-5 NB & SB Lewis R	lver Brand N	NB Tou	utle River Br_	Deck Repair\1-CADD-Plans_Co	rtractPlans\22X333_PS_TCP.dgn			_	
TIME	10:01:50 AM				REGION STATE	FED.AID PROJ.NO.					l-5
DATE	3/14/2023			1 1	10 WASH	NHFP-0051(316)					
PLOTTED BY	hillb				IU WASE						NB AND SB LEWIS RVR BR
DESIGNED BY	T. PANKAU				JOB NUMBER	1				Washington State	DECK REPAIR / OVERLAY
ENTERED BY	CCEO				22X333					, -	DEON KEFAIR / OVERLAT
CHECKED BY	A. ROMANCHOCK				CONTRACT NO.	LOCATION NO.				Department of Transportation	
PROJ. ENGR.	S. FELL						DATE		DATE	_	TRAFFIC CONTROL PLAN
REGIONAL ADM	. C. FRANCIS	REVISION	DATE	BY			P.E. STAMP BOX	P.E. STAMP BO			

	MININ	IUW 3	HOULI	JEK I	APER	LENG	II -	L/3 (le	et)	
SHOULDER Posted Speed (mph)										
(feet)	25	30	35	40	45	50	55	60	65	70
8'	-	-	-	-	120	130	150	160	170	190
10'	-	-		-	150	170	190	200	220	240

SIGN SPACING	= X (1)	
FREEWAYS & EXPRESSWAYS	55 / 70 MPH	1500' ±
RURAL HIGHWAYS	60 / 65 MPH	800′ ±
RURAL ROADS	45 / 55 MPH	500' ±
(1) ALL SPACING MAY BE ADJUSTED TO RAMPS, AT-GRADE INTERSECTIONS AN		INTERCHANGE

CHANNELIZATION DEVICE SPACING (feet)									
MPH	TAPER	TANGENT							
50/70	40	80							
35/45	30	60							

BUFFER DATA											
LONGITUDINAL BUFFER SPACE = B											
SPEED (MPH)	25	30	35	40	45	50	55	60	65	70	
LENGTH (feet)	155	200	250	305	360	425	495	570	645	730	
TRANSPORTABLE ATTENUATOR ROLL AHEAD DISTANCE = R											
HOST VEHICLE WEIGHT 9,900 TO 22,000 lbs.					HOST VEHICLE WEIGHT > 22,000 lbs.						

< 45 MPH

74'

45-55 MPH

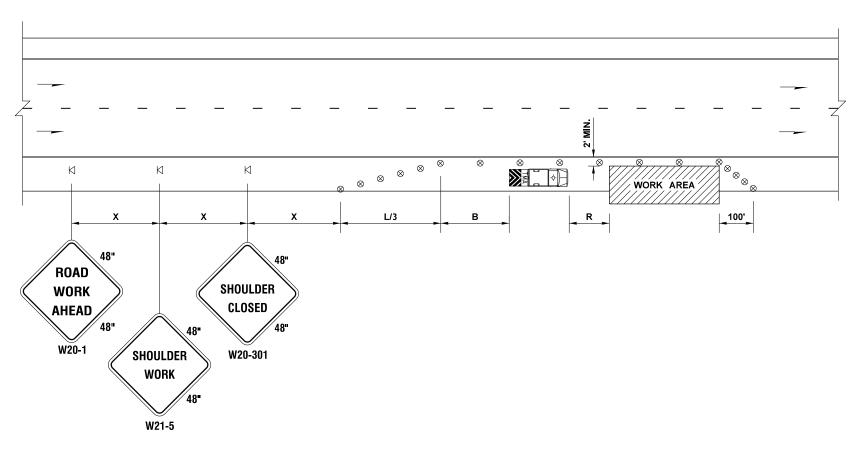
100'

> 55 MPH

150'

> 55 MPH

172'



LEGEND

- TEMPORARY SIGN LOCATION
- TRAFFIC SAFETY DRUM

TRANSPORTABLE ATTENUATOR

SHOULDER CLOSURE - HIGH SPEED

NOTES

< 45 MPH 45-55 MPH

123'

100'

- 1. NO ENCROACHMENT IN TRAVELED LANE IF ENCROACHMENT IS NECESSARY, LANE SHALL BE CLOSED.
- 2. DEVICE SPACING FOR THE DOWNSTREAM TAPER SHALL BE 20' (FT) O.C.
- 3. ALL SIGNS ARE BLACK ON ORANGE.

NOT TO SCALE

FILE NAME	\\Wsdot.loc\sw\Group\Englnee	ering\444301\05-Design\02-Projects\I-5 NB & SB Lewis	River Br and N	B Toutle RI	ver Br_D	eck Repair\1-CADD-Plans_Cor	tractPlans\22X333_PS_TCP.dgn				Plot 3
TIME	10:01:50 AM			REGION NO.	STATE	FED.AID PROJ.NO.				I-5	PLAN REF NO
DATE	3/14/2023			10	WASH	NHFP-0051(316)					TC3
PLOTTED BY	hillb			10	WASH					NB AND SB LEWIS RVR BR	
DESIGNED BY	T. PANKAU				NUMBER				Washington State	DECK REPAIR / OVERLAY	SHEET
ENTERED BY	CCEO			22X	(333				, ,	DEON INCI AIN TOVENEAT	84
CHECKED BY	A. ROMANCHOCK			CONTR	RACT NO.	LOCATION NO.			Department of Transportation		OF
PROJ. ENGR.	S. FELL						DATE	DATE	-	TRAFFIC CONTROL PLAN	84 SHEETS
REGIONAL ADM	C. FRANCIS	REVISION	DATE	BY			P.E. STAMP BOX	P.E. STAMP BOX			SHEETS